



**CITY OF SUNNYVALE  
REPORT  
Planning Commission**

**February 11, 2008**

**SUBJECT:**           **2007-0462 – Essex Property Trust** [Applicant] **DL Tasman LP** [Owner]: Application for related proposals on a 6.5-acre site located at **615 Tasman Drive** (at N. Fair Oaks Ave.) in a C-2/PD (Highway Business/Planned Development) Zoning District. (Negative Declaration) (APN: 110-29-038) JM;

Motion               **Special Development Permit** to allow a mixed-use development with 290 condominium/apartment units and 51,308 square feet of commercial use, and

Motion               **Vesting Tentative Map** to allow 290 residential condominium units and 16 commercial condominium units on one lot.

**REPORT IN BRIEF**

**Existing Site Conditions**           Two Office Buildings

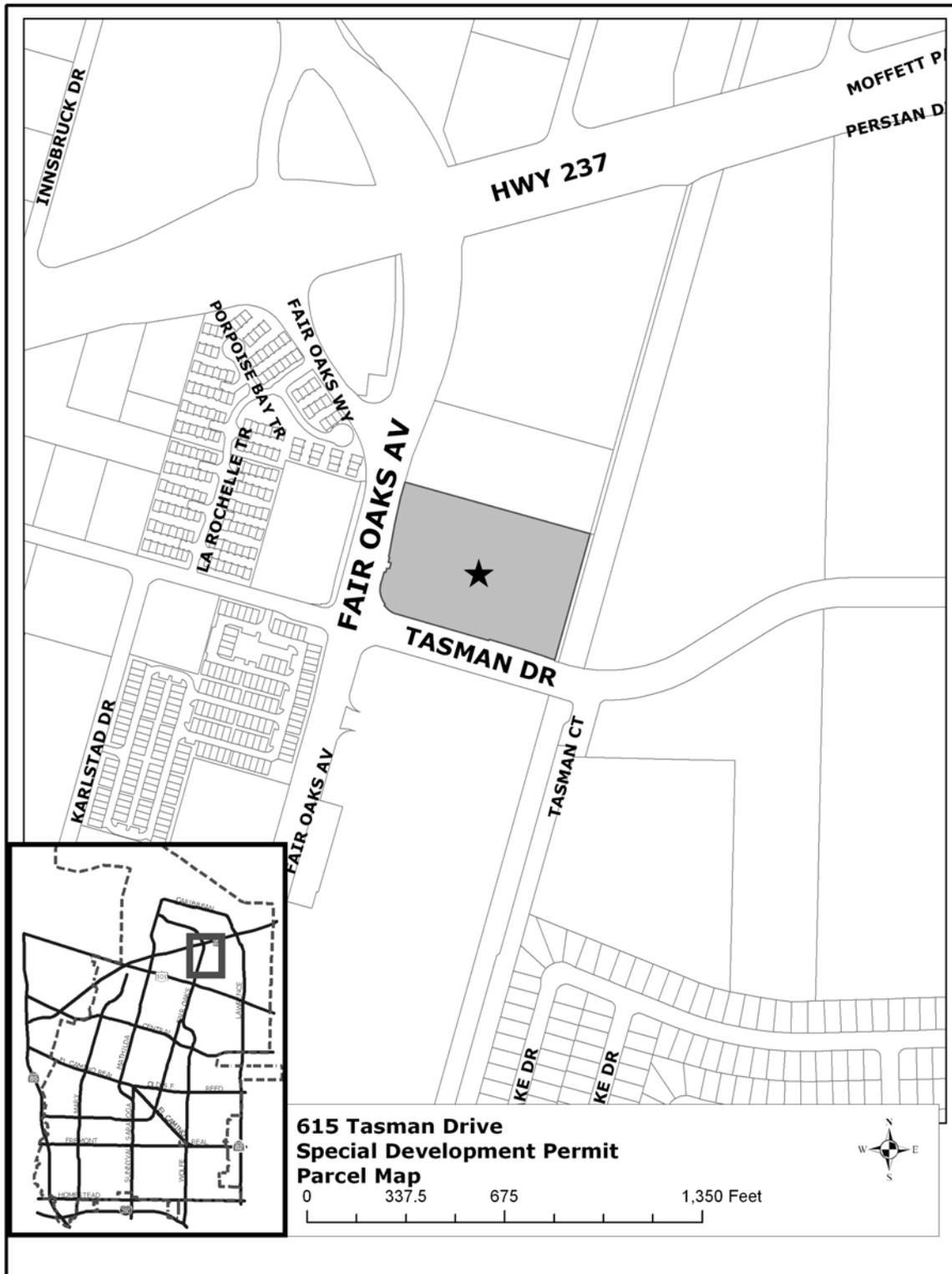
**Surrounding Land Uses**

North	High Density Apartments
South	Across Tasman Drive, Mobile Home Park
East	Across Water District Stream, Mobile Home Park
West	Across Fair Oaks Avenue, Commercial and Medium Density Residential

**Issues**                               Noise  
Architecture

**Environmental Status**           A Mitigated Negative Declaration has been prepared in compliance with California Environmental Quality Act provisions and City Guidelines.

**Staff Recommendation**           Approve with Conditions



**PROJECT DATA TABLE**

	<b>EXISTING</b>	<b>PROPOSED</b>	<b>REQUIRED/ PERMITTED</b>
<b>General Plan</b>	Industrial-to-Residential Med-High Density	Same	Industrial-to-Residential Med-High Density
<b>Zoning District</b>	C-2/PD	Same	C-2/PD
<b>Lot Size (s.f.)</b>	281,188 (6.45 ac)	Same	none
<b>Gross Floor Area (s.f.)</b>	106,000	366,905 (51,308 comm* & 315,597 res*)	none
<b>Lot Coverage (%)</b>	38%	32%	35% max.
<b>Floor Area Ratio (FAR)</b>	38%	152%	N/A
<b>No. of Res. Units</b>	N/A	290	290 max.
<b>Density (units/acre)</b>	N/A	45	45 max.
<b>Meets 75% min?</b>	N/A	N/A	N/A
<b>Bedrooms/Unit</b>	N/A	160 x 1-bedrm 130 x 2-bedrm	N/A
<b>Unit Sizes (s.f.)</b>	N/A	694-916 for 1-bed 1043-1225 for 2-bed	N/A
<b>Lockable Storage / Unit (cu.ft.)</b>	N/A	300 (in garage)	300 min.
<b>No. of Buildings On-Site</b>	2	2	---
<b>Distance Between Buildings</b>	Approx. 25	242	25 min.
<b>Building Height (ft.)</b>	Approx. 25	75	75 max.
<b>No. of Stories</b>	1	6	8 max.
<b>Setbacks (Facing Property)</b>			
<b>Front (Fair Oaks)</b>	Approx. 70	13 (0 at corner)	70 comm. 20 res. min.
<b>Front (Tasman)</b>	Approx. 40	12' 9" (0 at corner)	15 min.**
<b>Left Side</b>	Approx. 150	48	25 min.***
<b>Rear</b>	Approx. 50	35	25 min.***



	EXISTING	PROPOSED	REQUIRED/ PERMITTED
<b>Landscaping (sq. ft.)</b>			
<b>Total Landscaping</b>	Approx. 50,000	128,917	108,750 min.
<b>Landscaping/Unit</b>	N/A	445	375 min.
<b>Total Usable Open Space</b>	N/A	118,611	110,200 min.
<b>Usable Open Space/Unit</b>	N/A	409	380 min.
★ <b>Frontage Width (ft.)</b>	15	0 to 13	15 ft. min.
<b>Buffer Adj. Residential (ft.)</b>	10	30	10 ft. min.
★ <b>Parking Lot Area Shading (%)</b>	Approx. 15%	20% of 150 uncovered spaces	50% min. in 15 years
<b>Water Conserving Plants (%)</b>	Approx. 30%	70%	70% min.
<b>Parking</b>			
<b>Total Spaces</b>	Approx. 212	731 (228 comm & 503 res)	728 (228 comm & 500 res) min.
<b>Standard Spaces</b>	212	686	514 min.
<b>Compact Spaces/ % of Total</b>	0	45	199 (10% comm & 35% res) max.
<b>Accessible Spaces</b>	7	15	15 min.
<b>Covered Spaces</b>	0	582	290 min.
<b>Aisle Width (ft.)</b>	26	26	26 min.
<b>Bicycle Parking</b>	0	9 x 10-bike bike racks (secured)	46 Class II and 106 Class I min.

★ Starred items indicate deviations from the Sunnyvale Municipal Code requirements.

\* Abbreviations of “comm” = commercial and “res” = residential.

\*\* Non-major street (standard is 70 feet).

\*\*\* Due to height of building.

## **ANALYSIS**

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### **Description of Proposed Project**

Essex Property Trust has proposed a mixed use development at 615 Tasman Drive, at the northeast corner of the intersection with North Fair Oaks Avenue. The project consists of 290 residential units (315,597 square feet) and 51,308 square feet of commercial use (15.7% of the total area).

The site plan includes a one-story approximately 15,000 square foot commercial building near the intersection and a one-story commercial building with five-stories of residential on second through sixth floors. The residential units are setback from the commercial units to provide a sense of separation between the uses. Parking is located on the ground floor and in one floor of subsurface parking.

The 51,308 square feet of commercial use includes a 15,000 square foot full service grocery store located at the corner and other service retail located around the perimeter of the main parking lot. The grocery store is an important component of the development as it addresses the City Council's long-standing concern for the limited neighborhood retail and the need for a grocery store in the residential area. The applicant has a letter of intent from a grocery store chain, and the store has already made a public news release regarding their intent to move to the location.

The residential buildings have been broken up into three U-shaped buildings to reduce the appearance of massing and provide the visual sense of smaller segments. The 290 residential units include 44 Below Market Rate (BMR) units, which complies with the Sunnyvale Municipal Code requirement of 15% for apartments. The design of the site is a Mediterranean-style architecture.

A Vesting Tentative Map is included as part of the application. The applicant has stated the intent to retain ownership and rent both the residential and commercial units. The Map allows for the potential of future conversion of the rental units to for-sale units.

### **Background**

The area was re-zoned in 2004 as part of Tasman/Fair Oaks Area Pedestrian and Bicycle Circulation Plan. The re-zoning of the area included a target density for the overall area, which the project meets. During the re-zoning process, the City Council discussed the need for the emerging residential area (Industrial-to-Residential Area #8) to have a full service grocery store in this area. The subject site was identified as an optimal location.

**Planning Actions on the Site:** The following table summarizes previous relevant planning applications related to the subject site.

<b>File Number</b>	<b>Brief Description</b>	<b>Hearing / Decision</b>	<b>Date</b>
2004-0454	To Rezone from R-4/PD to C-2/PD (ITR 8) and Pedestrian Circulation /Transit Oriented Development Guidelines	Approved by the City Council	8/10/04
2002-0116	Council Study Issue to consider a General Plan Amendment to change Industrial zoning to Residential and Retail in the Area bounded by SR 237, US-101 and Lawrence Expressway.	Approved by the City Council; subject site re-zoned to R-4/PD	7/16/02

### **Environmental Review**

A Mitigated Negative Declaration has been prepared in compliance with the California Environmental Quality Act provisions and City Guidelines. An initial study has determined that the proposed project would not create any significant environmental impacts with implementation of the recommended mitigation measures (see Attachment C, Initial Study).

The Mitigated Negative Declaration called for installing windows and glass doors rated a minimum Sound Transmission Class (STC) of 25 within 220 feet of both Tasman Drive and Fair oaks Avenue to address noise from the streets and Light Rail.

### **Special Development Permit**

**Site Layout:** The subject site is located in a growing residential area with Mobile Home Parks located south and east of the site and an emerging Industrial-to-Residential area located west and southwest of the site. High density apartments are located north of the site, and a small commercial center is located just west of the site across North Fair Oaks Avenue. The increase in housing in the area has underscored the demand for a neighborhood retail center and grocery store in the area.

Access to the site is both enhanced and challenged by the Light Rail line that runs along the southern and western boundaries of the site. While it enhances public transportation access to the mixed use site, it limits the ability of southbound vehicles on North Fair Oaks and eastbound vehicles on Tasman

Drive from turning directly onto the site. Traffic flows to and from the site were studied as part of a transportation study conducted in 2002 as part of a General Plan Amendment resulting from a Council Study Issue.

The primarily L-shaped site configuration has the commercial and residential buildings set back along the northern and eastern portions of the site, allowing for appropriate setbacks and buffers from the neighboring properties. The layout configuration also includes an approximately 15,000 square foot retail building, proposed as a full service grocery store, which is located along the southwestern corner of the site.

The proposed project includes six-stories (75 feet high) above one story of sub-surface parking. The commercial space and additional parking are on the ground floor, with the residential units and amenities located on the second through sixth floors.

The project for the residential area includes a swimming pool and a 1,700 square foot community room in addition to other community-centered amenities in the common areas.

*VTA Opportunity Zone.* The project site is also located within ¼ mile of a light rail transit station, which is defined by the Valley Transportation Authority (VTA) as an opportunity zone for transit-oriented development. VTA has several recommended design guidelines, such as internal pathways providing convenient pedestrian access through the project site to the transit stop, which facilitate the use of mass transit. Staff will continue to work with the applicant to incorporate these guidelines where feasible.

Transit oriented design encourages features such as pedestrian circulation throughout the site and information kiosks on site or on adjacent right-of-way.

*Setbacks.* The six-story buildings have a 48-foot setback along the northern and 35-feet along the eastern boundaries of the site which serves as a fire lane and buffer from the neighboring residential and Santa Clara Valley Water District properties. The front yard setbacks along Tasman and Fair Oaks are generally in compliance with the front yard setback and buffer requirements, although the measurement is complicated by the inconsistent property line along Tasman Drive and Fair Oaks Avenue. This inconsistency is due to the modification in property lines that occurred when the Light Rail was installed in the 1990s. The stand-alone retail building located on the southwest portion of the site does not meet the 70-foot front yard setbacks required in commercial zoning districts. While the building is close to the property line at the corner, the existing right turn lane resulted in a modified corner which has reduced the corner frontage area. The proposed structure does, however, meet the Vision Triangle requirements. Locating retail buildings close to the

sidewalk is encouraged in transit oriented developments by the VTA Community Design and Transportation Best Practices Manual as it makes the site more inviting to pedestrians.

A shading study, provided by the applicant, indicates that the shading of neighboring roofs will be minimal and the solar access requirements of limiting the shading to a maximum of 10% of the roof areas shall be met.

*Easements and Undergrounding:* All onsite and offsite utilities will need to be undergrounded, per standard Municipal Code requirements. There is an existing public easement which runs through the site which may be abandoned. Due to its location near a transit station, the City is requesting a ten-foot sidewalk and an associated easement. (The easement is not consistently ten-foot, given that the property line is not linear but jogs toward and away from the street at different points.) The City is also requesting the standard ten-foot Public Utility Easement for this project.

The following Guidelines were considered in analysis of the project site design.

Citywide Design Guidelines	Comments
<p><i>Site Design. New development shall adhere to the character of the existing neighborhood and be integrated into the surrounding development. New development shall not dominate or interfere with the established character of its neighborhood. Site design of projects shall be cohesive both functionally and visually.</i></p>	<p>Although the adjacent property is three-story or less, this site zoned Commercial and is well positioned to be major mixed use site. It is located at a major intersection and adjacent to a major public transportation (Light Rail) stop.</p>
<p><i>B1. Locate site components such as structures, parking, driveways, walkways, landscaping and open spaces to maximize visual appeal and functional efficiency.</i></p>	<p>The project was designed to provide a cohesive center, designed to maximize the use of the area, and provide accessibility to all businesses in the retail area.</p>

The project complies with several recommended guidelines from the VTA Community Design and Transportation Best Practices Manual including minimizing building setbacks, locating public uses on the ground floor, providing human scale for pedestrians, avoiding blank walls, and design of parking areas minimize the impact of parking and avoid compromising the pedestrian environment.

**Architecture:** The six-story buildings consist of commercial tenant spaces and parking on the ground floor and five-stories of residential units located above, with one-story of subsurface parking.

The architecture evolved from a more modern appearance to the current Mediterranean architecture style, based on input from the City’s architecture consultant as well as from Planning Commissioners during Study Sessions on October 8, 2007 and on January 14, 2008. The proposed Mediterranean architecture includes accented colors and balconies. The applicant has provided four-sided architecture, given the high level of visibility of the project from all perspectives.

Staff has worked with the applicant to provide a visual break between the commercial and residential portions of the site and has requested architectural features and a setback (at least five feet) between the planes of the two types of developments.

The following Guidelines were considered in the analysis of the project architecture.

<b>Design Policy or Guideline (Architecture)</b>	<b>Comments</b>
<i>B1. Break up large buildings into groups of smaller segments whenever possible, to appear smaller in mass.</i>	The residential portion is broken into 3 different buildings, which allows for a visual sense of smaller segments and a sense of space between the buildings.
<i>B12 – Develop focal points for every project to create a sense of identification. Plazas, landscaping, water features, art works, changes in pavement textures and levels, and building architectural features may be combined to create focal points.</i>	The commercial project uses a mix of awnings and architectural features to provide the appearance of multiple storefronts and a separation between residential and commercial. The art feature located at the corner of the property will be a focal point on the buildings or at the intersection of Tasman Drive and Fair Oaks Avenue.

**Landscaping:** The existing site has several large trees located throughout the site and along the northern boundary. Staff worked with the applicant to maximize the number of mature trees on-site to maintain a buffer from the proposed project and the existing residential located just north of the site. The

challenge with maintaining the existing trees is the required grade change to meet the Building Code requirements related to the flood plain.

Of the 85 trees located on the site, 61 are protected trees. (Protected trees are those that measure 38 inches or greater in circumference when measured at four and a half feet from the ground.) The plans for this project include preserving 9 of the protected trees. The rationale provided for the tree removal is due to the location of the buildings or proposed changes to the existing grade. Staff is working with the applicant to explore opportunities to maintain more trees along the northern boundary of the site.

The proposed landscaping plan includes approximately 205 new trees. Staff has worked with the applicant to address the loss of the existing trees by requiring larger replacement trees on the site that are native species that will enhance the appearance of the project.

The following Guidelines were considered in analysis of the project landscaping.

<b>Design Policy or Guideline (Landscape)</b>	<b>Comments</b>
<i>IV Landscaping shall be used to enhance sites and buildings, control climate and noise, create transition between adjacent uses, unify various site components, and define and separate functions.</i>	The landscaping plan provides for visual variety on the site, parking lot shading, and enhancement of the residential common areas.
<i>Citywide Design Guidelines A2. Preserve and incorporate existing natural features, particularly trees, on a site into the landscape design of projects.</i>	A certified arborist report indicated that some of the existing trees along the northern boundary can be maintained on the site with the implementation of adequate protection measures during construction.
<i>Citywide Design Guidelines A6. Preserve natural site features such as mature trees, creeks, views, etc. and incorporate into the site design of the new project (Tree Preservation Ordinance)</i>	Staff is working with the applicant to preserve as many of the existing mature trees as possible, especially along the northern boundary of the site, while meeting the applicant’s needs for the site.

*Parking Lot Shading:* The proposed site layout results in 582 of the 731 total parking spaces being covered given that they are located within a parking

structure. For the remaining 149 spaces, the proposed landscaping results in only about 20% shading. The 50% shading requirement is specific to the open paved parking in order to reduce the heat island effect. Staff recommends requiring additional trees in the uncovered parking area would increase shading and provide an additional visual buffer from the residential units.

**Parking:** The project meets the full parking requirement for residential and commercial without utilizing shared parking. The residential parking will be located below-grade and at grade along the eastern side of the development. The commercial parking is located at grade, in front of the commercial buildings as well as within the northern portion of the at-grade covered garage area located behind the commercial buildings.

The applicant has proposed a parking management plan which includes parking stickers and a security gate to limit access to the residential parking. The residential parking area will have 290 assigned and 213 unassigned spaces, which would be available for visitors. A parking plan will be developed to allow access to the unassigned residential parking spaces for guests. This may be accomplished by either relocating the security gate and providing signage which designates the parking as guest-only, or installing a telecommunication device which enables a visitor to call up to the resident's apartment to be granted access. Residents will have either a code or access card. The parking lots will be monitored by parking staff. After pre-determined time thresholds, resident's cars may be towed or ticketed if they park in the retail area.

In a shopping center, there are no separate parking requirements for specific use. Parking is determined using a "shopping center" rate. Proposed restaurant uses that occupy more than the 10% threshold of the center may be considered through a Miscellaneous Plan Permit process, if it is determined that the character of the site has changed from a shopping center.

**Circulation:** The corner site provides for access challenges due to the existing Light Rail tracks which run along both the Tasman and Fair Oaks frontages. The applicant explored a connection route with the neighboring site to the north to access the traffic light; unfortunately, the neighboring property owner was not interested in a shared access option.

The circulation onsite provides for access to the commercial buildings from one ingress/egress point on Tasman Drive and two ingress/egress points on Fair Oaks Avenue. The resident-only parking areas may be accessed from Tasman Drive or through the three ingress/egress points for the commercial area.

Pedestrian access will be facilitated by the required sidewalks along Tasman and Fair Oaks, and by linking to propose on-site sidewalks to encourage

pedestrian traffic flows. The *Tasman/Fair Oaks Pedestrian and Bicycle Circulation Plan* includes a proposed pedestrian-friendly remodeling of the Tasman-Fair oaks intersection which is anticipated to be completed in the next several years, but is not a part of this project. This project will contribute to the pedestrian upgrades through a “Sense of Place” fee.

**Below-Market Rate (BMR) Housing:** This project complies with the 15% BMR requirement for rental housing. The 15% rate is now in effect due to the apartment vacancy rate dropping below 3% in a 2007 survey (SMC 19.66.020(a)).

Although the project includes a Vesting Tentative Map, the applicant has stated the intent to operate the project as an apartment complex for the near future. However, it is possible for the project to convert to condominiums at any time after the filing of this map. For this reason, a legal agreement will be created with the developer to ensure certain procedural requirements (such as possible modification to number of BMR units, compliance with Condominium Conversion section) to be met at the time the owner decides to sell the units individually.

*Transition from Apartments to Ownership Units.* If and when a condominium map is exercised and there is a change from rental to ownership, the project will be affected in three ways:

1. Change in the required below-market rate units (BMRs). If in the future the dwelling units are sub-divided and sold as separate ownership units, the BMR requirement for the ownership units at the time of the application will apply.
2. Triggers the Condominium Conversion section of the Subdivision Map Act when apartments are ultimately sold to individuals.
3. Requires the filing of CC&Rs with the final map.

The methods for transitioning from the 15% BMR rate for apartments to the current BMR rate for for-sale units shall be in accordance with the terms of Sunnyvale Municipal Code 19.66 and be detailed in a Deed restriction on the property.

**Green Building Features:** The applicant has stated their commitment to the Leadership in Energy and Environmental Design (LEED) Green Building design intent and has identified key areas that will enable the project to achieve 26 LEED points, which would qualify the project as LEED certified for new construction and for commercial buildings. (Residential buildings require a minimum of 45 points to be LEED certified, and the pilot checklist for the recently released Neighborhood Design requires 40 points for a project to be LEED certified.)

Some of the proposed measures include: higher density of development, pedestrian access to basic services such as groceries and retail, open space, bicycle storage, reduced "heat-island" effects on roof areas, minimized surface parking, designated parking for low-emitting/fuel efficient vehicles, providing dual-glazing to reduce energy loads, incorporating renewable and recycled materials in the design, incorporating low-emitting materials in the finishes, energy-efficient appliances, reduced light pollution, native species-water conserving landscaping, and energy conserving devices for lighting of common areas.

**Art in Private Development:** This project is subject to the Art in Private Development Standards in the Municipal Code. While the type of art has not yet been determined, the proposed options are to either incorporate it into the architecture or locate it along the Tasman Drive or Fair Oaks Avenue street frontages. The application should submit an Art in Private Development application to the Director of Community Development for approval by the Arts Commission, prior to issuance of a Building Permit.

**Compliance with Development Standards/Guidelines:** The SMC does not have setbacks established for mixed use developments. The project does not meet the front yard setbacks or frontage requirements due to the location of the stand-alone commercial building located on the southwest corner of the site. The proposed 152% Floor Area Ratio (FAR) exceeds the 55% FAR for the C-2 zoning district.

*Stormwater Management:* The project triggers stormwater management requirements. The applicant has submitted a draft Stormwater Management Plan (SWMP) that includes stormwater filters and bio-retention facilities. The project will require a final SWMP to be certified by a pre-qualified third party.

*Guidelines and Standards for Land Use near Streams:* The eastern perimeter of the subject site abuts the Sunnyvale East Channel, which is owned by the Santa Clara Valley Water District (SCVWD). Although Sunnyvale is responsible for administering the guidelines, Sunnyvale staff conferred with SCVWD staff on this project. The *Guidelines and Standards* will be met by providing the proposed setbacks, using native species landscaping, and providing a well-engineered design of the subsurface parking adjacent to the Channel (factoring in the soil type and depth to groundwater in the design). Lighting along the Channel should be minimized, with no spillage of light onto the Channel.

*The Tasman/Fair Oaks Pedestrian and Bicycle Circulation Plan* identifies the site as a pedestrian destination and calls for new sidewalks along Tasman Drive. A link will need to be constructed to connect the new sidewalk along Tasman Drive to the pathway on the east side of the Santa Clara Valley Water

District (SCVWD) property on Tasman, which will require coordination with the SCVWD.

*Residential Transportation Demand Management (TDM).* The City has a Council policy to promote a range of transportation options in the City. The City also endorses location high-density development along major transportation corridors and transit lines. The *Tasman/Fair Oaks Light Rail Corridor and ITR 7 and 8* area is a targeted action area for implementation of TDM in the City. The required site design techniques for TDM are included as conditions of approval.

**Expected Impact on the Surroundings:** The proposed development is expected to increase traffic flow to and from the subject site, as well as increase lighting and noise levels. Given that the site is located along a major transportation corridor, heavier traffic flow and additional lighting and noise may be expected in this area.

The proposed retail, including a full service grocery store, will be an asset to this new residential area that has grown considerably in the past few years.

### **Vesting Tentative Map**

The applicant has proposed a Vesting Tentative Map for the 290 residential and six commercial units to enable the future option of sub-dividing the property into air-space commercial and residential condominium units. The applicant has stated that the subdivision is not anticipated in the near future, but the Vesting Tentative Map is to provide for future ownership options.

### **Fiscal Impact**

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This project involves a Transportation Impact Fee of approximately \$225,485.45 to address the expected resulting traffic impacts from the change in use. The project also includes a Park In-Lieu Fee of approximately \$2,728,598.40 (at \$9,408.96 per unit), and a Sense of Place Fee of approximately \$209,000 (at \$1,000 per unit).

The business from the retail component of the project is expected to positively impact the City's tax revenue.

### **Public Contact**

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Given the size of the project, expanded noticing of the public hearing was mailed to neighboring residents beyond the standard 300 feet required by City's standard mailing process.

**Planning Commission Study Session:** The Planning Commission reviewed the project during Study Sessions conducted on October 8, 2007 and January 14, 2008. The first study session provided an overview of the project but focused primarily on the architecture, the second focused on architecture, circulation and tree preservation. Concerns with the architecture included style, coloring, and the need for greater distinction between the residential and commercial aspects of the site. The applicant modified the elevations based on the comments made by the Planning Commissioners.

There was some discussion about vehicle and pedestrian access to and from the site. Concerns were expressed with the access limitations caused by the Light Rail tracks. A request was made to distinguish the walkways which cross the parking lot through the use of distinctive pavers to facilitate the on-site pedestrian traffic.

**Neighborhood Meeting:** A neighborhood meeting was held on January 16, 2008 in the evening. Approximately 24 residents attended to review the project. Attendees included residents from Traditions townhomes, Fox Hollow, El Dorado, and Plaza Del Rey mobile home park. Questions were asked regarding parking, signage, pedestrian flow, number of restaurants, bike rack locations, and construction impacts. Enthusiasm was expressed for the proposed grocery store.

<b>Notice of Negative Declaration and Public Hearing</b>	<b>Staff Report</b>	<b>Agenda</b>
<ul style="list-style-type: none"> <li>• Published in the <i>Sun</i> newspaper</li> <li>• Posted on the site</li> <li>• 500 notices mailed to the property owners and residents within approx. 1,000 ft. of the project site</li> </ul>	<ul style="list-style-type: none"> <li>• Posted on the City of Sunnyvale's Website</li> <li>• Provided at the Reference Section of the City of Sunnyvale's Public Library</li> </ul>	<ul style="list-style-type: none"> <li>• Posted on the City's official notice bulletin board</li> <li>• City of Sunnyvale's Website</li> </ul>

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**Conclusion**

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**Discussion:** The project is expected to provide additional housing to an area identified as transitioning to residential as well as provide needed retail to the area, including a full service grocery store.

Staff worked with the applicant to preserve the existing mature trees located along the northern boundary of the site, and is suggesting that an additional

10 trees be required to be preserved, allowing for removal only if an on-site arborist verifies that they cannot be saved during site grading.

The proposed 152% Floor Area Ratio (FAR) exceeds the 55% FAR for the C-2 zoning district. However, the unique expectations for the site provide a rationale for exceeding the maximum FAR.

The current site plan suggests an encroachment into the front yard setbacks and landscape frontage by the stand-alone retail building located on the southwest portion of the site, which has been designated as a grocery-type use. There is some precedence in the city for retail centers to take advantage of the visibility offered by the corner location and thus to accept reduced setbacks. Staff is recommending a slightly modified design which rounds the two corners of the building that are closest to the corner to take it out of the landscaping frontage area. Staff considers reduced setbacks appropriate for retail, given the proposed use as a full service grocery store, which benefit from a greater street presence.

The proposed green features are considered to be an enhancement to the design. As proposed, the site would meet LEED certification design intent for commercial buildings. It is of note that many of the proposed features are required by the new Building Code.

**Findings and General Plan Goals:** Staff was able to make the required Findings based on the justifications for the Special Development Permit and Tentative Map. Findings and General Plan Goals are located in Attachment A.

**Conditions of Approval:** Conditions of Approval are located in Attachment B.

## **Alternatives**

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1. Adopt the Mitigated Negative Declaration and approve the Special Development Permit and Tentative Map with attached conditions.
2. Adopt the Mitigated Negative Declaration and approve the Special Development Permit and Tentative Map with modified conditions.
3. Adopt the Mitigated Negative Declaration and deny the Special Development Permit and Tentative Map.
4. Do not adopt the Mitigated Negative Declaration and direct staff as to where additional environmental analysis is required.

**Recommendation**

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Alternative 1.

Prepared by:

Jamie McLeod  
Senior Planner

Reviewed by:

Gerri Caruso  
Principal Planner

Reviewed by:

Trudi Ryan  
Planning Officer

Attachments:

- A. Recommended Findings
- B. Recommended Conditions of Approval
- C. Mitigated Negative Declaration
- D. Site and Architectural Plans
- E. Vesting Tentative Map
- F. List of Justifications from the Applicant

## **Recommended Findings - Special Development Permit**

Goals and Policies that relate to this project are:

### **Citywide Design Guidelines**

*B1. Locate site components such as structures, parking, driveways, walkways, landscaping and open spaces to maximize visual appeal and functional efficiency.*

The project was designed to provide a cohesive center, designed to maximize the use of the area, and provide accessibility to businesses in the shopping center.

*B2. Emphasize the pleasant components of the project such as existing trees and views, and disguise its less desirable scenes such as loading service areas through placement and design of structures and landscaping.*

The layout is designed to create aesthetic focal points on the buildings, and staff is working with the applicant to maximize the retention and use of the existing mature trees on the northern boundary of the site.

### **Council Policy Manual**

*1.1.15 Residential Transportation Demand Management*

*3. Tasman / Fair Oaks Light Rail Corridor and ITR 7 and 8*

*The light rail corridor provides the greatest opportunity for transit access to a multitude of Silicon Valley's high tech employers in Sunnyvale, Santa Clara, San Jose, and Mountain View. The location is also conducive to bicycling and walking due to its close proximity to major Sunnyvale Employment areas. Industrial to Residential (ITR) designations are in place to the east of the Fair Oaks Tasman Light Rail Station. Enhanced bicycle and pedestrian connectivity and design guidelines were also adopted for the area in 2004. High density and commercially zoned portions of ITR 7 and 8 are required to further implement TDM per this policy. The city is supportive of developing high-density housing within the ITR residential areas as well as in conjunction with substantial commercial development at the intersection of Fair Oaks Avenue and Tasman Drive. However, the commercial zoning shall be maintained to provide needed services for the neighborhood and shall not be changed to exclusive residential development without identifying an equal alternative commercial location to serve the neighborhood.*

The Project complies with the policy for encouraging residential use in the Tasman / Fair Oaks area and providing neighborhood supporting commercial development.

**Design Policy or Guideline (Architecture)**

*B12. Develop focal points for every project to create a sense of identification. Plazas, landscaping, water features, art works, changes in pavement textures and levels, and building architectural features may be combined to create focal points.*

The commercial project uses a mix of architectural styles to give the appearance of multiple storefronts.

*B1. Break up large buildings into groups of smaller segments whenever possible, to appear smaller in mass.*

The residential buildings have been broken up into three U-shaped buildings to reduce the appearance of massing and provide the visual sense of smaller segments.

**Design Policy or Guideline (Landscape)**

*Citywide Design Guidelines A6. Preserve natural site features such as mature trees, creeks, views, etc. and incorporate into the site design of the new project (Tree Preservation Ordinance)*

Staff is working with the applicant to preserve as many of the existing mature trees as possible along the northern property line while meeting the applicant's needs for the site.

*IV Landscaping shall be used to enhance sites and buildings, control climate and noise, create transition between adjacent uses, unify various site components, and define and separate functions.*

The landscaping plans provide for visual enhancement of the site, parking lot shading, and a privacy barrier for the existing residential north of the site.

*A2 Preserve and incorporate existing natural features, particularly trees, on a site into the landscape design of projects.*

Staff is recommending that a certified arborist be on site during construction to identify opportunities to protect and retain mature trees.

1. The proposed use attains the objectives and purposes of the General Plan of the City of Sunnyvale [Finding made]. The project meets policy goals for residential development in the Tasman/Fair Oaks area and provides for the needed commercial center. The proposed full service grocery store is a major benefit to this growing residential area. The residential component is set back slightly from the commercial use and the quality architectural provides for a strong sense of presence at a prominent intersection in the City. The higher density is compatible with the zoning for the area.
  
2. The proposed use ensures that the general appearance of proposed structures, or the uses to be made of the property to which the application refers, will not impair either the orderly development of, or the existing uses being made of, adjacent properties [Finding made]. The project appearance meets the Citywide Design Guidelines and the architectural standards. As conditioned, the site design is expected to address concerns for noise.

**Recommended Findings - Vesting Tentative Map**

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In order to approve the Vesting Tentative Map, the proposed subdivision must be consistent with the general plan. Staff finds that the Vesting Tentative Map is in conformance with the General Plan. However, if any of the following findings can be made, the Vesting Tentative Map shall be denied. Staff was not able to make any of the following findings and recommends approval of the Vesting Tentative Map.

1. That the subdivision is not consistent with the General Plan.
2. That the design or improvement of the proposed subdivision is not consistent with the General Plan.
3. That the site is not physically suitable for the proposed type of development.
4. That the site is not physically suitable for the proposed density of development.
5. That the design of the subdivision or proposed improvements is likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.
6. That the design of the subdivision or type of improvements is likely to cause serious public health problems.
7. That the design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of property within the proposed subdivision.
8. That the map fails to meet or perform one or more requirements or conditions imposed by the "Subdivision Map Act" or by the Municipal Code

Staff was not able to make any of the findings (B.1-8), and recommends approval of the Vesting Tentative Map.

**Recommended Conditions of Approval - Special Development Permit and Vesting Tentative Map**

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In addition to complying with all applicable City, County, State and Federal Statutes, Codes, Ordinances, Resolutions and Regulations, Permittee expressly accepts and agrees to comply with the following conditions of approval of this Permit. Unless otherwise noted, all conditions shall be subject to the review of approval of the Director of Community Development.

**1. GENERAL CONDITIONS**

- A. Project shall be in conformance with the plans approved at the public hearing. Any major site and architectural plan modifications shall be treated as an amendment of the original approval and shall be subject to approval at a public hearing except that minor changes of the approved plans may be approved by the Director of Community Development without a public hearing.
- B. The Conditions of Approval shall be reproduced on the cover page of the plans submitted for a Building permit for this project. An annotated set of conditions indicating how they are being satisfied must be submitted with the Building Permit application.
- C. The Special Development Permit shall be null and void two years from the date of approval by the final review authority at a public hearing if the approval is not exercised, unless a written request for an extension is received and approved prior to expiration date.
- D. The Vesting Tentative Map shall be null and void two years from the date of approval by the final review authority at a public hearing if the approval is not exercised, unless a written request for an extension is received and approved prior to expiration date. Per City Code, the applicant has the option of three one-year extensions.
- E. Execute a Special Development Permit document prior to issuance of the building permit.
- F. Any expansion or modification of the approved use shall be approved by separate application at a public hearing by the Planning Commission.
- G. Staging, storage of construction materials and equipment, construction trailers, contractor parking and deliveries of materials must be done on-site. The Public Right of Way shall be kept free and clear of any and all construction related equipment and debris.
- H. Occupancy for the residential units shall not be permitted prior to the Building Permits for the commercial uses being finalized.

**2. COMPLY WITH OR OBTAIN OTHER PERMITS**

- A. Obtain necessary permits from other Divisions and Department for all proposed on-site and off-site improvements.
- B. To address storm water runoff pollution prevention requirements, an Impervious Surface Calculation worksheet is required to be completed and submitted for the California Regional Water Quality Control Board prior to issuance of a Building Permit.
- C. The civil and geotechnical plans need to take the proximity of the adjacent levee into consideration and be designed accordingly, taking the soil and groundwater conditions into consideration.
- D. Obtain approval from the Crime Prevention Division of the Public Safety Department for crime prevention measures appropriate to the proposed development prior to issuance of a Building Permit.
- E. Coordinate with the Santa Clara Valley Water District for feedback on the design of the subsurface parking lot to ensure design will not jeopardize their conveyance system.

**3. ENVIRONMENTAL MITIGATION MEASURES**

- A. In addition to complying with applicable City Codes, Ordinances, and Resolutions, the following mitigation measures are incorporated into the project to minimize the identified potential environmental impacts:
- B. Submit plans for approval by the Director of Community Development showing the design and method by which continuous or recurrent noise will be buffered per the 8 May 2007 acoustical report, prepared on by Edward L. Pack Associates, Inc. which provided the following mitigations to enable the project to meet the City of Sunnyvale's Noise Element requirements for residences that states the indoor noise level should not exceed DNL 45 dB and outdoor noise levels should not exceed DNL 60 dB:

WHAT:

Exterior Noise Controls (to achieve 60 dB)

- 1) Construct 42 inch high acoustically-effective balcony railings at all balconies within 220 feet of the centerline of Tasman Drive and with a direct or side view of the roadway.
- 2) Construct 42 inch high acoustically-effective balcony railings at all balconies within 160 feet of the centerline of North Fair Oaks Avenue and with a direct or side view of the roadway.

(Note: The balcony railing height is in reference to the nearest balcony floor elevation. Additional detail on the balcony railing is provided in the acoustical report.)

Interior Noise Controls (to achieve 45 dB)

- 1) Install windows and glass doors rated a minimum Sound Transmission Class (STC) of 25 for all windows and doors within 220 feet of the centerline of Tasman Drive and with a direct or side view of the roadway. Provide mechanical ventilation to units, and inform occupants that they need to maintain closed windows and doors to achieve the noise standard.
- 2) Install windows and glass doors rated a minimum Sound Transmission Class (STC) of 25 for all windows and doors within 160 feet of the centerline of North Fair Oaks Avenue and with a direct or side view of the roadway. Provide mechanical ventilation to units, and inform occupants that they need to maintain closed windows and doors to achieve the noise standard.

(Note: Per State and City Building Codes, adequate ventilation shall be required for habitable rooms with closed windows if outdoor noise exposure is LDN 60 DBA or greater. A sufficient HVAC system is required. The system shall not compromise noise isolation.)

WHEN: These mitigation measures will be converted into conditions of approval for this Special Development Permit prior to its final approval. The conditions will become valid when the SDP is approved and prior to building permit issuance.

WHO: The developer is responsible for completing all of the above mitigation measures.

HOW: These measures shall be clearly marked and demonstrated in the plans submitted for building permits.

- C. Final construction drawings shall incorporate all noise mitigation measures as set forth under "Mitigation Measures."
- D. Provisions of Title 25 of the California Administrative Code shall be satisfied with dependence on mechanical ventilation.
- E. Final plans shall bear the signature of the Acoustical consultant.
- F. Acoustical tests shall be performed by the developer to demonstrate that an interior Ldn scale (day and night average noise level) of 45 dBA is met on the finished units. Such test results shall be furnished to the Director of Community Development prior to

occupancy of the units. Corrective measures shall be implemented if noise level exceeds the standard.

**4. CONDOMINIUM CONVERSION REQUIREMENTS**

- A. Comply with Conversion of Rental Housing to Community Housing Project requirements as noted in SMC 19.70.
- B. Prior to conversion, the applicant shall submit all information listed under 19.70.040(d) Use Permits and Special Development permits at least 18 months prior to the individual sale of units for review and approval by the Director of Community Development.
- C. Prior to conversion, the applicant shall demonstrate compliance all requirements of 19.70.050 Minimum Requirements, which includes provisions for Tenant Protection, Buyer Protection, and Site Improvements and Amenities.

**5. BMR (BELOW MARKET RATE UNITS)**

- A. Comply with Below Market Rate Housing (BMR) requirements as noted in SMC 19.66.
- B. The project will provide 44 Below Market Rate rental dwelling units in compliance with SMC 19.66.
- C. If and when the project transitions from rental to for-sale units, the project shall comply with Municipal Code requirements for ownership BMR units. The process for the transition shall be detailed in the Deed restriction document, to be completed and recorded with the County prior to issuance of Building Permits. The text for the Deed restriction document shall be provided by the City's Housing Division.
- D. The developer shall submit a site plan to the Housing Officer for review. The plan will include a description of the number, type, size and location of each unit on the site. The Housing Officer will then determine the specific units to be obligated as Below Market Rate (BMR) units (BMR Administrative Guidelines).
- E. Prior to issuance of a building permit, the developer shall execute a Development Agreement with the City to establish the units. The rental price of the BMR units is established at the time of the execution of the Development Agreement. (BMR Administrative Guidelines).
- F. All BMR dwelling units shall be constructed concurrently with non-BMR units, and shall be dispersed throughout the property and shall reflect the range in numbers of bedrooms provided in the total project and shall not be distinguished by exterior design, construction or materials. (SMC 19.66.020(c))

- G. Sixty days (60) days prior to the estimated occupancy date, the developer shall notify the Housing Division of the BMR units to be available. (BMR Administrative Guidelines)
- H. BMR rental units - Record a "Deed of Trust" Prior to Occupancy Permit. (BMR Administrative Guidelines)
- I. The original rental price of BMR dwelling units shall comply with sales prices established by the City, which is revised annually. (SMC 19.66.040 (c))
- J. Below Market Rate dwelling units shall be offered for rent only to persons qualified under the terms described in SMC 19.66.040 and 19.66.050 and described more fully in the Administrative Guidelines. (BMR Rental Units)
- K. In the event of any material breach of the Below Market Rate Program requirements and conditions, the City may institute appropriate legal actions or proceedings necessary to ensure compliance. (SMC 19.66.140)
- L. In the event that any of the Below Market Rate dwelling units or a portion thereof is destroyed by fire or other cause, all insurance proceeds therefrom shall be used to rebuild such units. Grantee hereby covenants to cause the City of Sunnyvale to be named additional insured party to all fire and casualty insurance policies pertaining to said assisted units. (BMR Administrative Guidelines)

**6. CC&R's (CONDITIONS, COVENANTS AND RESTRICTIONS) - required when Vesting Tentative map exercised**

- A. Any proposed deeds, covenants, restrictions and by-laws relating to the subdivision are subject to review and approval by the Director of Community Development and the City Attorney.
- B. The developer/Owner shall create a Homeowner's Association that comports with the state law requirements for Common Interest Developments. Covenants, conditions and restrictions (CC&Rs) relating to the development are subject to approval by the City Attorney and Director of Community Development prior to approval of the Final Map. In addition to requirements as may be specified elsewhere, the CC&R's shall include the following provisions:
- C. Membership in and support of an association controlling and maintaining all common facilities shall be mandatory for all property owners within the development.
- D. The homeowners association shall obtain approval from the Director of Community Development prior to any modification of the CC&R's pertaining to or specifying the City.

- E. The developer shall maintain all utilities and landscaping for a period of three years following installation of such improvements or until the improvements are transferred to a homeowners association, following sale of at least 75% of the units, whichever comes first.
- F. The Conditions of Approval of this Special Development Permit.
- G. The CC&Rs shall contain the following language:
1. "Right to Remedy Failure to Maintain Common Area. In the event that there is a failure to maintain the Common Area so that owners, lessees, and their guests suffer, or will suffer, substantial diminution in the enjoyment, use, or property value of their Project, thereby impairing the health, safety and welfare of the residents in the Project, the City, by and through its duly authorized officers and employees, will have the right to enter upon the subject Property, and to commence and complete such work as is necessary to maintain said Common Area. The City will enter and repair only if, after giving the Association and Owners written notice of the failure to maintain the Common Area, they do not commence correction of such conditions in no more than thirty (30) days from the giving of the notice and proceed diligently to completion. All expenses incurred by the City shall be paid within thirty (30) days of written demand. Upon a failure to pay within said thirty (30) days, the City will have the right to impose a lien for the proportionate share of such costs against each Lot in the Project.
  2. It is understood that by the provisions hereof, the City is not required to take any affirmative action, and any action undertaken by the City will be that which, in its sole discretion, it deems reasonable to protect the public health, safety and general welfare, and to enforce it and the regulations and ordinances and other laws.
  3. It is understood that action or inaction by the City, under the provisions hereof, will not constitute a waiver or relinquishment of any of its rights to seek redress for the violation of any of the provisions of these restrictions or any of the rules, regulations and ordinances of the City, or of other laws by way of a suit in law or equity in a court of competent jurisdiction or by other action.
  4. It is further understood that the remedies available to the City by the provision of this section or by reason of any other provisions of law will be cumulative and not exclusive of the

maintenance of any other remedy. In this connection, it is understood and agreed that the failure to maintain the Common Area will be deemed to be a public nuisance and the City will have the right to abate said condition, assess the costs thereof, and cause the collection of said assessments to be made on the tax roll in the manner provided by appropriate provisions of the Sunnyvale Municipal Code or any other applicable law.

5. No Waiver. No failure of the City of Sunnyvale to enforce any of the covenants or restrictions contained herein will in any event render them ineffective.
6. Third-Party Beneficiary. The rights of the City of Sunnyvale pursuant to this Article will be the rights of an intended third party beneficiary of a contract, as provided in Section 1559 of the California Civil Code, except that there will be no right of Declarant, the Association, or any Owner(s) to rescind the contract involved so as to defeat such rights of the City of Sunnyvale.
7. Hold Harmless. Declarant, Owners, and each successor in interest of Declarant and said Owners, hereby agree to save, defend and hold the City of Sunnyvale harmless from any and all liability for inverse condemnation which may result from, or be based upon, City's approval of the Development of the subject Property."

## **7. COMMERCIAL USE**

- A. All uses permitted within a C-1 Zoning District (SMC 19.20.030) shall be permitted on this property, except that the following uses shall require a separate Special Development Permit:
  1. Sit-down restaurants with hours after 10:00 p.m.
  2. Entertainment uses
  3. Except as noted below establishments for which a general liquor license is required
  4. Except as noted below establishments for which an on-sale liquor license is required
  5. Dog grooming/Kennel
- B. The commercial uses shall include a minimum of an approximately 15,000 square foot area for a full service grocery store, providing sales of foodstuffs, including fresh produce and meat.

- C. The approximately 15,000 square foot full service grocery store may sell beer and wine (for off-site use only). The beer and wine sales are only permitted in conjunction with the standard grocery sales. Sale of liquor or hard alcohol shall require review through a Miscellaneous Plan Permit process.

**8. DESIGN/EXTERIOR COLORS AND MATERIALS**

- A. The plans shall be revised to be consistent with the plans approved at the public hearing. Final exterior building materials and color scheme are subject to review and approval of the Director of Community Development prior to issuance of a building permit. Exterior materials shall be of high quality and designed to appeal to pedestrian access.
- B. Roof material shall be tile or equivalent, as approved by the Director of Community Development.
- C. Fabric awnings on the commercial storefronts shall be replaced at least every five years or earlier if quality is greatly deteriorated. Any change of color, materials or design and are subject to review and approval by the Director of Community Development.
- D. Provide at least five feet of setback and architectural features between the plane of the commercial and residential units. The residential units shall have the appearance of being distinct from the commercial portion of the development.
- E. The entryway on the den in the one-bedroom residential units (unit Plan 1C) shall be larger than a standard door size and no door shall be allowed on the den, unless adequate parking can be demonstrated with the unit calculated as a 2-bedroom unit.
- F. Modify retail building, located at intersection, to allow for 15 foot setback. This may be accomplished by rounding the building corners.

**9. EXTERIOR EQUIPMENT**

- A. Individual air conditioning units shall be screened with architecture or landscaping features to be considered by the Director of Community Development through a Miscellaneous Plan Permit process. Noise levels shall be mitigated as required for separate condominium units.
- B. Any modification or expansion of unenclosed uses shall require approval from the Director of Community Development.

- C. All unenclosed materials, equipment and/or supplies of any kind shall be maintained within approved enclosure area. Any stacked or stored items shall not exceed the height of the enclosure.

**10. TRANSIT-ORIENTED DEVELOPMENT**

- A. Provide for the following site development features:
  - 1. Information kiosks on site or on adjacent right-of-way,
  - 2. On-site rideshare and carpool contact information,
- B. Submit a pedestrian facility design plan that creates a hierarchy of pathways oriented to the following:
  - 1. Access to living units,
  - 2. Pedestrian circulation throughout the site,
  - 3. Public street access, and
  - 4. Designated exclusive pathway connections to sidewalks.

Approval of the plan by the Director of Community Development is required prior to issuance of a building permit. The plan shall identify specific design features, e.g. pathway materials, pathway width, lighting style, associated landscaping that identify each path hierarchy. Ensure that the plan facilitates pedestrian access and connection to the public street at the northeast corner of the site.

- C. Continue to work with Staff on a modified site plan that incorporates the following design features:
  - 1. Raised/textured pavements,
  - 2. Curb ramps,
  - 3. Lighting at intersections of pedestrian pathways and drive aisles,
  - 4. Designated carpool loading areas on site, and
  - 5. Ten foot sidewalk at the public street frontages with street trees, street tree wells, tree grates, ornamental/pedestrian scale lighting, and other street furniture as approved by the Directors of Public Works and Community Development.

Approval of the modified site plan by the Director of Community Development is required prior to issuance of a building permit.

- D. Continue to work with Staff to further develop features to introduce human activity along the street frontages.
- E. Provide a cost assessment for participation in the Valley Transportation Authority EcoPass Residential Program or a like

program from a major transit service provider to provide transit passes for residents of the facility.

**11. ART IN PRIVATE DEVELOPMENT**

- A. Comply with the art in private development requirements as noted in Sunnyvale Municipal Code Section 19.52.
- B. Provide publicly visible artwork along Tasman Drive or Fair Oaks Avenue. Art may be stand-alone or incorporated into the architecture.
- C. Submit an Art in Private Development application to the Director of Community Development for approval by the Arts Commission, prior to issuance of a Building Permit.

**12. FEES**

- A. Pay Traffic Impact fee estimated at \$225,485.45, prior to issuance of a Building Permit. (SMC 3.50)
- B. Pay Sense of Place fee estimated at \$209,000, prior to issuance of a Building Permit.
- C. Pay Park In-lieu fees estimated at \$2,728,598.40, prior to approval of the Final Map or Vesting Tentative Map. (SMC 18.10) or Building Permit, whichever first occurs.
- D. Pay Art in Private Development Bond estimated at 1% of the commercial project costs, as determined at issuance of Building Permits. (SMC 19.52)

**13. LANDSCAPING**

- A. Landscape and irrigation plans shall be submitted to the Director of Community Development subject to approval by the Director of Community Development prior to issuance of a Building Permit. Landscaping and irrigation shall be installed prior to occupancy.
- B. Screen loading area adjacent to retail building, located at intersection, with adequate landscaping.
- C. Provide decorative paving as required by the Director of Community Development to distinguish entry driveways, building entries, pedestrian paths and common areas. Install decorative street lights and street trees per Tasman/Fair Oaks Area Pedestrian and Bicycle Circulation Plan.
- D. Provide separate meter for domestic and irrigation water systems.

- E. All landscaping shall be installed in accordance with the approved landscape plan and shall thereafter be maintained in a neat, clean, and healthful condition.
- F. Trees shall be allowed to grow to the full genetic height and habit (trees shall not be topped). Trees shall be maintained using standard arboriculture practices.
- G. Provide trees at minimum 30 feet intervals along side and rear property lines, except where mature trees are located immediately adjoining on neighboring property.
- H. Any “protected trees” (as defined in SMC 19.94) approved for removal shall be replaced with a specimen tree of at least 36-inch box size.
- I. All remaining new trees installed, other than replacement of protected trees, shall be 24-inch box size or larger.
- J. No invasive landscaping shall be permitted to be part of the landscaping plan. Native species landscaping, as defined in the *Guidelines and Standards for Land Use Near Streams*, are strongly encouraged for the landscaping. Drought-tolerant species are also encouraged (at least 70%).
- K. Trees for the site shall be native species, as large as appropriate for the site, unless determined by the City Arborist to be unfeasible for the specific location.
- L. At the expense of the subdivider, City staff shall install required street trees of a species determined by the Public Works Department. Obtain approval of a detailed landscape and irrigation plan from the Director of Community Development (SMC 19.38.070) prior to issuance of a Building Permit.
- M. Provide a 15-foot wide landscape buffer along the southern and western property line.
- N. Provide a tree-shading plan that accommodates 50% shading within 15 years of planting for all parking and maneuvering areas on the site.
- O. Ground cover shall be planted so as to ensure full coverage eighteen months after installation.
- P. All areas not required for parking, driveways or structures shall be landscaped.

#### **14. TREE PRESERVATION**

- A. Prior to issuance of a Demolition Permit, a Grading Permit or a Building Permit, whichever occurs first, obtain approval of a tree

protection plan from the Director of Community Development. Two copies are required to be submitted for approval. The Plan shall include, at a minimum, 6-foot high chain-link fencing around the drip line of the trees, and no construction debris or equipment is stored within the fenced area during the course of demolition and construction.

- B. The tree protection plan shall be installed prior to issuance of any Building Permits, subject to the on-site inspection and approval by the City Arborist.
- C. The tree protection plan shall remain in place for the duration of construction.
- D. Overlay Civil plans including utility lines to ensure that the tree root system is not damaged.

## **15. FENCES**

- A. Design and location of any proposed fencing and/or walls are subject to the review and approval by the Director of Community Development.
- B. Install and maintain an eight foot decorative masonry wall, measured from the highest adjoining grade, of a design approved by the Director of Community Development along the northern and eastern property lines. (Wherever the grade differential is one foot or higher, a concrete or masonry retaining wall shall be installed.)
- C. Only fences, hedges and shrubs or other natural objects 3 feet or less in height may be located within a "vision triangle" (SMC 19.12.040(16) and SMC 19.12.050 (12)).

## **16. LIGHTING**

- A. Prior to issuance of a Building Permit submit an exterior lighting plan, including fixture and pole designs, for approval by the Director of Community Development. Driveway and parking area lights shall include the following:
  - 1. Sodium vapor (or illumination with an equivalent energy savings).
  - 2. Pole heights to be uniform and compatible with the areas, including the adjacent residential areas. Light standards shall not exceed 15 feet on the interior of the project and 8 feet in height on the periphery of the project near residential uses.
  - 3. Provide photocells for on/off control of all security and area lights.

4. All exterior security lights shall be equipped with vandal resistant covers.
  5. Wall packs shall not extend above the roof of the building.
  6. Lights shall have shields to prevent glare onto adjacent and on-site residential properties, as well as preventing glare on the stream along the eastern boundary of the site.
- B. Prior to issuance of a Building Permit submit a contour photometric plan for approval by the Director of Community Development.

**17. ON-SITE AMENITIES**

- A. Swimming pools, pool equipment structures, play equipment and other accessory structures, except as otherwise subject to Planning Commission review, shall be reviewed by the Director of Community Development prior to issuance of building permits and subject to approval of design, location and colors.
- B. The swimming pool shall be heated using a solar hot water system.

**18. PARKING**

- A. All residential assigned, residential unassigned and commercial parking areas shall be clearly marked. Each residential unit shall have at least one assigned covered parking space.
- B. Parking areas shall be maintained at all times so as to allow for parking of vehicles and shall be enforced by property owner or owners association.
- C. Unenclosed storage of any vehicle intended for recreation purposes, including land conveyances, vessels and aircraft, but excluding attached camper bodies and motor homes not exceeding 18 feet in length, shall be prohibited on the premises.

**Parking Lot**

- D. All compact spaces shall be clearly marked in order to discourage larger cars from parking in smaller spaces.
- E. Parking lot striping and marking (e.g. compact, guest) shall be accurately and adequately maintained.
- F. Signs shall be required to direct vehicles to additional parking spaces on-site.
- G. Adequate lighting plan shall be available in parking lots to keep them safe and desirable for use.
- H. A maximum of 10% compact parking of unassigned spaces shall be permitted.

**Residential – Parking Management**

- I. Submit a Parking and Circulation Management Plan for the residential area to the Director of Community Development for review and approval prior to issuance of a Building Permit to ensure compliance with parking requirements and describing how property managers or homeowner's associations meet the following requirements:
  1. Limit the amount of unassigned spaces that are reserved for individual use to 290,
  2. Give property managers/homeowner's association the latitude to define "guest," since ultimate enforcement is the responsibility of that entity,
  3. 50% of unassigned spaces be reserved for guest use only, at the discretion of the property owner or homeowners association,
  4. Note that property owners and HOA's cannot rent unassigned spaces, except that a nominal fee may be charged for parking management,
  5. Require tenants to use their assigned parking spaces prior to using the unassigned parking spaces,
  6. Confirm the responsibility of the property owner or homeowner's association to enforce provisions of the parking management plan.

**Commercial – Required/Allowable Parking**

- J. Restaurants using the 100% fixed seating rate may not provide less than the retail or the shopping center rate.
- K. The parking rate for restaurants with 100% fixed seats shall be for limited seating situations.
- L. All restaurants shall receive a staff level permit in order for the parking to be evaluated as part of the review process.

**Commercial – Parking Management**

- M. Submit a Parking and Circulation Management Plan for the commercial area to the Director of Community Development for review and approval prior to issuance of a Building Permit to ensure compliance with parking requirements and describing how property managers or homeowner's associations meet the following requirements:
  1. That employee parking locations be away from the building, in parking spaces that are the least used,

2. The location and term of short-term parking.
- N. The property owner shall be responsible for enforcing provisions of the parking management plan.

**19. BICYCLE PARKING**

- A. Provide Class I and Class II Bicycle Parking, or the equivalent, per VTA Bicycle Technical Guidelines. As detailed in the site plans: Building 1 - 34 Class I and 7 Class II. Building 2 - 34 Class I and 7 Class II. Building 3 - 30 Class I and 6 Class II. Retail - 8 Class I and 26 Class II.
- B. Spaces shall be readily visible and located per VTA Bicycle Technical Guidelines or as approved by the Director of Community Development.

**20. RECYCLING AND SOLID WASTE**

- A. Submit a detailed recycling and solid waste disposal plan to the Director of Community Development for approval. Redesign configuration for adequate sizing (for both residential and commercial), easy access, and staging area in parking lot.
- B. Provide an adequate solid waste enclosure for the stand-alone approximately 15,000 square foot retail space to accommodate a grocery-type use. A compactor and cardboard baler-type facility may help minimize the size required. The design shall match the design, materials, color and be incorporated into the main building.
- C. All exterior recycling and solid waste shall be confined to approved receptacles and enclosures.

**21. EASEMENTS AND DEDICATIONS**

- A. Provide a ten-foot Public Utility Easement and a sidewalk easement for the ten-foot sidewalk.
- B. Remove the existing public easement that runs through the site prior to issuance of a Building Permit or Final Map, if deemed appropriate by the Director of Public Works.

**22. RIGHT-OF-WAY IMPROVEMENTS**

- A. Obtain a Permit from the Department of Public Works for Right-of-Way improvements.
- B. Curbs, gutters, sidewalks, streets, utilities, traffic control signs, electroliers (underground wiring) shall be designed, constructed and/or installed in accordance with City standards prior to occupancy and approved by the Director of Public Works.

- C. Comply with all Public Works requirements for right-of-way improvements.
- D. The developer shall work with the Valley Transit Authority in establishing bus stops, shelters, pads and turnout locations, if desired by the VTA.
- E. Work with VTA to develop the landscaping adjacent to the existing Light Rail stop to restrict potential jaywalking.

**23. ROOF/ROOF SCREENS**

- A. Roof vents, pipes and flues shall be combined and/or collected together on slopes of roof or behind parapets out of public view to meet code requirements as noted in Sunnyvale Municipal Code Section 19.38.020.

**24. SIGNS**

- A. A Master Sign Program shall be required for the commercial area. All signs shall be in conformance with and approved Master Sign Program. Residential signage shall conform to the Municipal Code and be compatible with other signs, or approved by the Director of Community Development.

**25. STORAGE**

- A. Provide 300 cu. ft. of lockable storage for each residential unit.

**26. TRAFFIC/ROAD IMPROVEMENTS**

- A. Approval of detailed street improvements plan shall be obtained from Public Works and bonds posted prior to issuance of a Building Permit. Improvements shall include but not be limited to:
  - 1. Streetlights will require separate Street lighting Plan with new conduits, pullboxes, conductors, Street light service cabinet and 120/240v PG&E service.
  - 2. Remove and replace existing curb ramp on Northeast corner with City Standard ADA curb ramp.
  - 3. Install truncated dome warning pads on northeast corner triangular island and north median island.
  - 4. All driveways to meet City design standards (Tasman/Fair Oaks Area Pedestrian and Bicycle Circulation Plan).
  - 5. Modification of existing driveway locations will require modifying signage on the median islands on Fair Oaks and Tasman

6. Proposed east driveway on Tasman is located after blind curve, must be entrance only no exit.
  7. Replace existing pedestrian signals with Countdown Pedestrian Signals at the traffic signal at Tasman/Fair Oaks. 2 on Northeast corner, 1 on northwest corner, and 1 on southeast corner.
  8. Remove and replace any sections of damaged, cracked, failing AC roadway on Tasman and Fair Oaks along project frontage.
  9. Slurry seal Tasman and Fair Oaks along project frontage to median island.
  10. Prepare separate Signing and Striping Plan.
  11. Install enhanced crosswalk treatment for east and north pedestrian crossings.
  12. Traffic control plan will be required for any and all work that may impact the Public Right of Way. Plan will be submitted with improvement plans.
  13. Provide 11' Right of Way from face of curb and 10' Public Utility Easement
  14. Conduct Vehicle Turning Analysis to show that a Sunnyvale DPS Ladder Truck will be able to access the site. Transportation and Traffic will supply AutoTurn template of vehicle to be modeled for use with AutoCad drawings.
- B. Complete other traffic impact mitigations, as identified by previous Traffic Impact Analysis or staff traffic analysis.

## **27. UNDERGROUND UTILITIES**

- A. All proposed utilities and utilities in the public right-of-way and on the site shall be undergrounded, including the Cable TV box on Fair Oaks by north property line (or may be relocated behind face of building).
- B. Applicant shall provide a copy of an agreement with affected utility companies for undergrounding of existing overhead utilities which are on-site or within adjoining rights-of-way prior to issuance of a Building Permit or a deposit in an amount sufficient to cover the cost of undergrounding shall be made with the City.
- C. If any additional poles are proposed to be added, developer shall have PG&E submit the preliminary plan to Public Works Department for review. City Council shall make the decision if any additional poles are acceptable or not. Under no circumstances shall additional poles be permitted along the frontage of this development.

- D. Install conduits along frontage for Cable TV, electrical and telephone lines in accordance with standards required by utility companies, prior to occupancy. Submit conduit plan to Planning Division prior to issuance of a Building Permit.
- E. Conduit sizing and locations shall be included on street improvement plans. Submit one copy to the Planning Division.
- F. Solar-assisted hot water shall be provided for all swimming pools and spas. Provide 70% of hot water needs for summer months.
- G. The recreation facilities (pool and clubhouse) shall be installed in connection with the first phase.

**28. VESTING TENTATIVE MAP CONDITIONS**

- A. Full development fees shall be paid for each project Vesting Tentative or lot shown on the Vesting Tentative Map and the fees shall be calculated in accordance with City Resolutions current at the time of payment.
- B. Comply with all applicable code requirements as noted in the Standard Development Requirements.

Project #: 2007-0462 Project Address: 615 Tasman Drive Applicant: Essex Property Trust
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Project Title	Essex Mixed Use Project at Tasman and Fair Oaks
Lead Agency Name and Address	City of Sunnyvale PO Box 3707 Sunnyvale, CA 94088-3707
Contact Person	Jamie McLeod, Senior Planner
Phone Number	(408) 730-7429
Project Location	615 Tasman Drive, Sunnyvale, CA
Project Sponsor's Name	Essex Property Trust
Address	615 Tasman Drive Sunnyvale, CA 94089-1707
Zoning	Highway Business / Planned Development (C-2/PD)
General Plan	Industrial to Medium Density Residential
Other Public Agencies whose approval is required	None

**Description of the Project**

The project consists of an application for a Special Development Permit for the construction of a mixed use development to construct 290 apartment units and 51,308 square feet of commercial use. The application includes a Parcel Map to allow 290 residential condominium units and 16 commercial condominium units on one lot. While the initial intent of the applicant is to develop apartment units, they want to reserve the entitlement rights (and will meet the associated requirements to) convert the apartments to separate ownership units at a future date.

The proposed layout of the site will have access off of Tasman Drive and Fair Oaks Avenue. The proposed building will be six-stories above ground with one-story of underground parking. The first floor will be commercial units facing the intersection with residential units located behind the commercial units. The residential area will contain a common area and swimming pool.

**Environmental Context**

The site is located in a primarily residential area, with mobile home parks located south and east of the site. Medium-high density apartment units are located north of the site, a commercial center is located west of the site across Fair oaks Avenue, and the area southwest of the site has converted from Industrial to Medium-density residential in the last few years. A stream is located just east of the site.

**EVALUATION OF ENVIRONMENTAL IMPACTS**

1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
3. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
4. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
5. "Negative Declaration: Potentially Significant Unless Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section 17, "Earlier Analysis," may be cross-referenced).
6. Earlier analysis may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c) (3) (d). In this case, a brief discussion should identify the following:
7. Earlier Analysis Used. Identify and state where they are available for review.
8. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
9. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project
10. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

**ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:**

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- |  |  |   |
|--|--|---|
| <input type="checkbox"/> Aesthetics                      | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Public Services                    |
| <input type="checkbox"/> Agricultural Resources          | <input type="checkbox"/> Hydrology/Water Quality       | <input type="checkbox"/> Recreation                         |
| <input type="checkbox"/> Air Quality                     | <input type="checkbox"/> Land Use/Planning             | <input type="checkbox"/> Transportation/Traffic             |
| <input checked="" type="checkbox"/> Biological Resources | <input type="checkbox"/> Mineral Resources             | <input type="checkbox"/> Utilities/Service Systems          |
| <input type="checkbox"/> Cultural Resources              | <input checked="" type="checkbox"/> Noise              | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Geology/Soils                   | <input type="checkbox"/> Population/Housing            |   |

**DETERMINATION: (To be completed by the Lead Agency)**

On the basis of this initial evaluation:

I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.

I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

I find that the proposed project MAY have a "potential significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect (1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.

I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

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Signature	Date
Jamie McLeod, Senior Planner	For the City of Sunnyvale (Lead Agency)

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
<b>1. AESTHETICS.</b> Would the project:					
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	2, 94, 111, see disc.
b. Substantially damage scenic resources, including, but not limited to trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 94, 111
c. Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 94, 111
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	2, 94, 111, see disc.
<b>2. AIR QUALITY:</b> Where available, significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:					
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	3, 94, 100, 111
b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	3, 94, 100, 111
c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
d. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
e. Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	111, 112
<b>3. BIOLOGICAL RESOURCES:</b>					
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U. S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
b. Have a substantially adverse impact on any riparian habitat or other sensitive natural community identified in local or regional	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion

**Environmental Checklist Form**

Project Address: 615 Tasman Drive, Sunnyvale

Applicant: Essex Property Trust

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
plans, policies, regulations, or by the California Department of Fish and Game or U.S Wildlife Service?					
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 94, 111, 112, 109
d. Interfere substantially with the movement of any resident or migratory fish or wildlife species or with established native resident migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 94, 111, 112, 109
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Conservation Community Plan, other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	41,94, 111, 112
<b>4. CULTURAL RESOURCES.</b> Would the project:					
a. Cause a substantial adverse change in the significance of a historical resource as defined in Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
b. Cause a substantial adverse change in the significance of an archaeological resources pursuant to Section 15064.5	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	10, 42, 94
c. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	10, 42, 94, 111
d. Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 111, 112
<b>5. LAND USE AND PLANNING.</b> Would the project:					
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 41, 94, 111

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
b. Conflict with an applicable land use plan, policy or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	31, 28, 111
c. Conflict with any applicable habitat conservation plan or natural communities conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 41, 94, 111
<b>6. MINERAL RESOURCES.</b> Would the project:					
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 94
b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 94
<b>7. NOISE.</b> Would the project result in:					
a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	See discussion
b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 16, 26, 94, 111, 112, 115
c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 16, 26, 94, 111, 112, 115
d. A substantially temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
<b>8. POPULATION AND HOUSING.</b> Would the project:					
a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion

Project Address: 615 Tasman Drive, Sunnyvale

Applicant: Essex Property Trust

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 11, 111, 112
c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 11, 111, 112
<b>9. PUBLIC SERVICES.</b> Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:					
a. Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
b. Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
c. Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
d. Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
e. Other services?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
<b>10. MANDATORY FINDINGS OF SIGNIFICANCE</b>					
a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 10, 26, 42, 59, 60, 61, 111, 112
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of the past projects, the effects of other current projects, and the effects of probable future projects)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	1, 2, 111, 112

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	111, 112
<b>11. GEOLOGY AND SOILS. Would the project:</b>					
a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:					
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	103 - 108
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	103 - 108
b. Result in substantial soil erosion or the loss of topsoil?					103 - 108
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	103 - 108
d. Be located on expansive soil, as defined in Table 18-a-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	103 - 108
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	103 - 108

Project Address: 615 Tasman Drive, Sunnyvale

Applicant: Essex Property Trust

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
<b>12. UTILITIES AND SERVICE SYSTEMS.</b> Would the project:					
a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 20, 24, 87, 88, 89, 90, 111, 112
b. Require or result in construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 20, 24, 25, 87, 88, 89, 111, 112
c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 20, 24, 25, 87, 88, 89, 111, 112
d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 20, 24, 25, 87, 88, 89, 111, 112
e. Result in a determination by the wastewater treatment provider which services or may serve the project determined that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 20, 24, 25, 87, 88, 89, 111, 112
f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 22, 90, 111, 112
g. Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 22, 90, 111, 112
<b>13. TRANSPORTATION/TRAFFIC.</b> Would the project:					
a. Cause an increase in the traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
b. Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 111, 112, 113
d. Substantially increase hazards to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g. farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 12, 71, 75-77, 80, 84, 111, 112
e. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 111, 112
f. Result in inadequate parking capacity?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	37, 111
g. Conflict with adopted policies or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 12, 81, 111, 112
<b>14. HAZARDS AND HAZARDOUS MATERIALS.</b>					
Would the project?					
a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the likely release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC
f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
g. Impair implementation of, or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC
h. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	UFC, UBC, SVMC
<b>15. RECREATION</b>					
a. Would the project increase the use of existing neighborhood or regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
<b>16. AGRICULTURE RESOURCES:</b> In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:					
a. Convert Prime Farmland, Unique Farmland or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	94
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	94
c. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	94

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
<b>17. HYDROLOGY AND WATER QUALITY.</b> Would the project:					
a. Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112
b. Substantially degrade groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112
d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or surface runoff in a manner which would result in flooding on- or off site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112
e. Create or contribute runoff which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See discussion
f. Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112
g. Place housing within a 100-year floodplain, as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112
h. Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112
i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112

Project Address: 615 Tasman Drive, Sunnyvale

Applicant: Essex Property Trust

	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source
j) Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	2, 24, 25, 111, 112

**DISCUSSION OF IMPACTS THAT ARE LESS THAN SIGNIFICANT**

**1. AESTHETICS**

(a) The proposed development may result in the view of the eastern hills being blocked for some residents. However, the proposed development falls within the framework of what is allowed by the zoning.

(d) The project would result in additional night lighting. However, the Municipal Code requires that project not result in light spillage on neighboring properties.

**2. AIR QUALITY**

(c), (d) The increase in density and use of the site, for both residential and commercial uses, will result in an increase in traffic and thus impact air quality. However, the additional vehicle trips are permitted by the zoning, and the public transportation LightRail stop located in front of the site on Tasman is expected to reduce the number of expected trips.

**3. BIOLOGICAL RESOURCES**

(a), (b), (d) The proposed project is located adjacent to a manmade channel created in the 1950 to convey stormwater to the San Francisco Bay. The channel is owned by the Santa Clara Valley Water District and both small fish and birds have been seen in the vicinity of the channel. The 2006 *Guidelines and Standards for Land Use Near Streams* was developed through a regional partnership of cities and the Water District to manage land use in the vicinity of streams. This project will comply with the requirements established in the *Guidelines and Standards*.

**7. NOISE**

(d) The project will introduce short-term and temporary additional sources of noise to the project area during construction. Through the City's implementation of the Municipal Code noise regulations, this impact will be lessened to a less than significant level during construction. The construction associated with this project would increase the noise levels in the neighborhood. Sunnyvale Municipal Code Section 16.08.150 regulates the hours of construction in order to reduce the noise impact on surrounding properties. Construction is permitted between the hours of 7:00 a.m. and 6:00 p.m. Monday through Friday, 8:00 a.m. to 5:00 p.m. on Saturday and construction is

prohibited on Sundays and national holidays. These restrictions are sufficient to reduce noise impact.

**8. POPULATION AND HOUSING (a)** This project will introduce 290 residential units where there is currently two commercial buildings. The city contains approximately 50,000 housing units, and this population increase is not substantial in relation to the existing population or adequacy of the infrastructure to support it.

**9. PUBLIC SERVICES**

**(a-e)** The proposed project will result in an increase in the use of all public facilities, including schools. The project will be required to pay school fees, as determined by the Sunnyvale Union Elementary and Fremont Union High School Districts. The project will also be expected to comply with the City's Park Dedication Fee requirement. The proposed project will generate funds for the citywide acquisition and improvement of park facilities to offset the increase of park facilities usage. Thus, this can be considered a less than significant impact.

**11. GEOLOGY AND SOILS**

**(a.ii), (a.iii)** The project site is not located in an area with any active faults, but may experience strong seismic ground shaking in the event of an earthquake. Through the City's implementation of the Uniform Building Code requirements for areas with potential for seismic activity this aspect of the project will be reduced to a less than significant level.

**12. UTILITIES AND SERVICE SYSTEMS**

The existing water and waste water systems have the capacity to accommodate the proposed project.

**13. TRANSPORTATION AND TRAFFIC**

**(a), (b)** The Traffic Division of the Public Works Department has determined the proposed project does not warrant the preparation of a Traffic Study and the project will not have a significant impact on transportation or traffic because it will not generate a notable number of additional peak hour traffic trips. The proposed use may be permitted under the existing zoning and General Plan designation,

**15. RECREATION**

**(a)** The impact of the project on the nearby parks and recreational facilities will be offset by the mitigation fees included as part of the conditions of approval for the project.

**(b)** The proposed development includes a swimming pool and recreational facility.

**17. HYDROLOGY AND WATER QUALITY**

(e) The development will be required to comply with the C.3 stormwater management requirements that will mitigate stormwater runoff from the site with regard to quality and peak quantity.

**DISCUSSION OF IMPACTS THAT ARE LESS THAN SIGNIFICANT WITH MITIGATION INCORPORATED:**

**7. NOISE (a)** The project will result in noise impacts on the residents of the project from the surrounding traffic and commuter LightRail train. The LightRail stop, located in front of the site on Tasman Drive, will have automated announcements.

**Acoustical Report**

An acoustical report, prepared on 8 May 2007 by Edward L. Pack Associates, Ince., was submitted by the applicant. The report noise evaluation is based on measurements taken at the site, acoustical criteria from the CA Code of Regulations and the Sunnyvale Noise Sub-Element. The City of Sunnyvale's Noise Element states that for residences, the indoor noise level should not exceed DNLI 45 dB and outdoor noise levels should not exceed DNL 60 dB.

As a result of the study, certain mitigation is necessary to meet interior noise levels for the new development.

**WHAT:**

Exterior Noise Controls (to achieve 60 dB)

- 1) Construct 42 inch high acoustically-effective balcony railings at all balconies within 220 feet of the centerline of Tasman Drive and with a direct or side view of the roadway.
- 2) Construct 42 inch high acoustically-effective balcony railings at all balconies within 160 feet of the centerline of North Fair Oaks Avenue and with a direct or side view of the roadway.

(Note: The balcony railing height is in reference to the nearest balcony floor elevation. Additional detail on the balcony railing is provided in the acoustical report.)

Interior Noise Controls (to achieve 45 dB)

- 1) Install windows and glass doors rated a minimum Sound Transmission Class (STC) of 25 for all windows and doors within 220 feet of the centerline of Tasman Drive and with a direct or side view of the roadway. Provide

mechanical ventilation to units, and inform occupants that they need to maintain closed windows and doors to achieve the noise standard.

- 2) Install windows and glass doors rated a minimum Sound Transmission Class (STC) of 25 for all windows and doors within 160 feet of the centerline of North Fair Oaks Avenue and with a direct or side view of the roadway. Provide mechanical ventilation to units, and inform occupants that they need to maintain closed windows and doors to achieve the noise standard.

(Note: Per State and City Building Codes, adequate ventilation shall be required for habitable rooms with windows shut if outdoor noise exposure is LDN 60 DBA or greater, as it is for the proposed development. A sufficient HVAC system is required, per the recommendation of a mechanical engineer. The system shall not compromise building shall noise isolation.)

**WHEN:** These mitigation measures will be converted into conditions of approval for this Special Development Permit prior to its final approval. The conditions will become valid when the SDP is approved and prior to building permit issuance.

**WHO:** The developer is responsible for completing all of the above mitigation measures.

**HOW:** These measures shall be clearly marked and demonstrated in the plans submitted for building permits.

**Jamie McLeod, Senior Planner**

**1/04/2008**

Completed By

Date

**Project Address: 615 Tasman Drive, Sunnyvale**

**Applicant: Essex Property Trust**

**City of Sunnyvale General Plan:**

- 2. Map
- 3. Air Quality Sub-Element
- 4. Community Design Sub-Element
- 5. Community Participation Sub-Element
- 6. Cultural Arts Sub-Element
- 7. Executive Summary
- 8. Fire Services Sub-Element
- 9. Fiscal Sub-Element
- 10. Heritage Preservation Sub-Element
- 11. Housing & Community Revitalization Sub-Element
- 12. Land Use & Transportation Sub-Element
- 13. Law Enforcement Sub-Element
- 14. Legislative Management Sub-Element
- 15. Library Sub-Element
- 16. Noise Sub-Element
- 17. Open Space Sub-Element.
- 18. Recreation Sub-Element
- 19. Safety & Seismic Safety Sub-Element
- 20. Sanitary Sewer System Sub-Element
- 21. Socio-Economic Sub-Element
- 22. Solid Waste Management Sub-Element
- 23. Support Services Sub-Element
- 24. Surface Run-off Sub-Element
- 25. Water Resources Sub-Element
  
- 26. **City of Sunnyvale Municipal Code:**
- 27. Chapter 10
- 28. Zoning Map
- 29. Chapter 19.42. Operating Standards
- 30. Chapter 19.28. Downtown Specific Plan District
- 31. Chapter 19.18. Residential Zoning Districts
- 32. Chapter 19.20. Commercial Zoning Districts
- 33. Chapter 19.22. Industrial Zoning Districts
- 34. Chapter 19.24. Office Zoning Districts
- 35. Chapter 19.26. Combining Zoning Districts
- 36. Chapter 19.28. Downtown Specific Plan
- 37. Chapter 19.46. Off-Street Parking & Loading
- 38. Chapter 19.56. Solar Access
- 39. Chapter 19.66. Affordable Housing
- 40. Chapter 19.72. Conversion of Mobile Home Parks to Other Uses
- 41. Chapter 19.94. Tree Preservation
- 42. Chapter 19.96. Heritage Preservation
- Specific Plans**
- 43. El Camino Real Precise Plan
- 44. Lockheed Site Master Use Permit
- 45. Moffett Field Comprehensive Use Plan
- 46. 101 & Lawrence Site Specific Plan
- 47. Southern Pacific Corridor Plan
  
- Environmental Impact Reports**
- 48. Futures Study Environmental Impact Report
- 49. Lockheed Site Master Use Permit Environmental Impact Report
- 50. Tasman Corridor LRT Environmental Impact Study (supplemental)
- 51. Kaiser Permanente Medical Center Replacement Center EIR (City of Santa Clara)

- 52. Downtown Development Program Environmental Impact Report
- 53. Caribbean-Moffett Park Environmental Impact Report
- 54. Southern Pacific Corridor Plan Environmental Impact Report

**Maps**

- 55. City of Sunnyvale Aerial Maps
- 56. Flood Insurance Rate Maps (FEMA)
- 57. Santa Clara County Assessors Parcel
- 58. Utility Maps (50 scale)

**Lists/Inventories**

- 59. Sunnyvale Cultural Resources Inventory List
- 60. Heritage Landmark Designation List
- 61. Santa Clara County Heritage Resource Inventory
- 62. Hazardous Waste & Substances Sites List (State of California)
- 63. List of Known Contaminants in Sunnyvale

**Legislation/Acts/Bills/Codes**

- 64. Subdivision Map Act
- 65. Uniform Fire Code, including amendments per SMC adoption
- 66. National Fire Code (National Fire Protection Association)
- 67. Title 19 California Administrative Code
- 68. California Assembly Bill 2185/2187 (Waters Bill)
- 69. California Assembly Bill 3777 (La Follette Bill)
- 70. Superfund Amendments & Reauthorization Act (SARA) Title III

**Transportation**

- 71. California Department of Transportation Highway Design Manual
- 72. California Department of Transportation Traffic Manual
- 73. California Department of Transportation Standard Plan
- 74. California Department of Transportation Standard Specification
- 75. Institute of Transportation Engineers - Trip Generation
- 76. Institute of Transportation Engineers Transportation and Traffic Engineering Handbook
- 77. U.S. Dept. of Transportation Federal Highway Admin. Manual on Uniform Traffic Control Devices for Street and Highways
- 78. California Vehicle Code
- 79. Traffic Engineering Theory & Practice by L. J. Pegnataro
- 80. Santa Clara County Congestion Management Program and Technical Guidelines
- 81. Santa Clara County Transportation Agency Short Range Transit Plan
- 82. Santa Clara County Transportation Plan
- 83. Traffic Volume Studies, City of Sunnyvale Public works Department of Traffic Engineering Division

**Project Address: 615 Tasman Drive, Sunnyvale**

**Applicant: Essex Property Trust**

- 84. Santa Clara County Sub-Regional Deficiency Plan
- 85. Bicycle Plan

**Public Works**

- 86. Standard Specifications and Details of the Department of Public Works
- 87. Storm Drain Master Plan
- 88. Sanitary Sewer Master Plan
- 89. Water Master Plan
- 90. Solid Waste Management Plan of Santa Clara County
- 91. Geotechnical Investigation Reports
- 92. Engineering Division Project Files
- 93. Subdivision and Parcel Map Files

**Miscellaneous**

- 94. Field Inspection
- 95. Environmental Information Form
- 96. Annual Summary of Containment Excesses (BAAQMD)
- 97. Current Air Quality Data
- 98. Chemical Emergency Preparedness Program (EPA) Interim Document in 1985?
- 99. Association of Bay Area Governments (ABAG) Population Projections

- 100. Bay Area Clean Air Plan
- 101. City-wide Design Guidelines
- 102. Industrial Design Guidelines

**Building Safety**

- 103. Uniform Building Code, Volume 1, (Including the California Building Code, Volume 1)
- 104. Uniform Building Code, Volume 2, (Including the California Building Code, Volume 2)
- 105. Uniform Plumbing Code, (Including the California Plumbing Code)
- 106. Uniform Mechanical Code, (Including the California Mechanical Code)
- 107. National Electrical Code (Including California Electrical Code)
- 108. Title 16 of the Sunnyvale Municipal Code

**Additional References**

- 109. USFWS/CA Dept. F&G Special Status Lists
- 110. Project Traffic Impact Analysis
- 111. Project Description
- 112. Project Development Plans
- 113. Santa Clara County Airport Land Use Plan
- 114. Federal Aviation Administration
- 115. Accoustical Analysis by Illingsworth & Rodkin, 2006



# 615 TASMAN DRIVE MIXED-USE (TASMAN DRIVE ELEVATION)

## PROJECT TEAM:

**DEVELOPER:** ESSEX PROPERTY TRUST  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
CONTACT: ANDREW BAKER  
TEL: 650-849-1707 FAX: 650-494-1871

**ARCHITECT/  
PLANNER:** KTG GROUP, INC.  
17892 MITCHELL SOUTH  
IRVINE, CALIFORNIA 92614  
CONTACT: RYAN YOSHIMOTO  
TEL: 949-851-2133 FAX: 949-851-8156

**CIVIL/  
LANDSCAPE:** HMH ENGINEERS  
1570 OAKLAND ROAD  
SAN JOSE, CALIFORNIA 95131  
CONTACT: AMIE ASHTON  
TEL: 408-487-2200 FAX: 408-487-2222

## INDEX OF DRAWINGS

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- T-2 SUN/SHADE ANALYSIS
- T-3 OPEN SPACE EXHIBIT
- SP-1 EXISTING SITE PLAN
- SP-2 PROPOSED SITE PLAN
- A-1 LOWER LEVEL GARAGE PLAN
- A-2 GROUND LEVEL COMPOSITE PLAN
- A-3 1ST FLOOR COMPOSITE PLAN
- A-4 2ND FLOOR COMPOSITE PLAN
- A-5 3RD FLOOR COMPOSITE PLAN
- A-6 4TH FLOOR COMPOSITE PLAN
- A-7 5TH FLOOR COMPOSITE PLAN
- A-8 ROOF PLAN
- A-9 EXTERIOR ELEVATIONS
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- A-11 EXTERIOR ELEVATIONS
- U-1 TYPICAL UNIT FLOOR PLANS
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- C-1 CONCEPTUAL GRADING & UTILITY PLAN
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- S-2 STORMWATER MANAGEMENT PLAN - DETAILS
- S-3 STORMWATER MANAGEMENT PLAN - DETAILS, SIZING CALCS.
- L-1 PRELIMINARY LANDSCAPE PLAN
- L-2 LANDSCAPE ENLARGEMENT PLAN
- L-3 LANDSCAPE ENLARGEMENT PLAN
- L-4 LANDSCAPE ENLARGEMENT PLAN
- L-5 LANDSCAPE DETAILS
- L-6 LANDSCAPE DETAILS
- L-7 TREE REMOVAL PLAN

## RESIDENTIAL, RETAIL & PARKING SUMMARY

PROJECT SUMMARY									
DESCRIPTION	AREA (G.A.P.)	PARKING RATIO	BLDG.			UNIT SUMMARY		TOTAL STALLS REQ.	TOTAL STALLS PROVIDED
			BLDG. 1	BLDG. 2	BLDG. 3	1 BR UNITS	2 BR UNITS		
PLAN 1A-1 BR/1BA	664	1.5	30	30	10	70		105	
PLAN 1B-1 BR/1BA	792	1.5	5	5	10	20		30	
PLAN 1C-1 BR/1BA	918	1.5	25	25	20	70		105	
PLAN 2A-2 BR/2BA	1043	2	15	15	35		65	130	
PLAN 2B-2 BR/2BA	1225	2	25	25	15		65	130	
RETAIL SPACE "A"	10,154	1/225						45	
RETAIL SPACE "B"	8,321	1/225						28	
RETAIL SPACE "C"	1,975	1/225						8	
RETAIL SPACE "D"	8,300	1/225						28	
RETAIL SPACE "E"	11,539	1/225						51	
RETAIL SPACE "F"	15,019	1/225						61	
TOTAL UNITS*	315,587		100	100	80	160	130	500	500
TOTAL RETAIL AREA	51,308							228	228
GRAND TOTAL	368,905						290	728	731

## PROJECT SUMMARY

FIVE STORY OVER TWO LEVEL PARKING STRUCTURE RESIDENTIAL MIXED-USE & SINGLE STORY RETAIL

TYPE III 1 HR. CONSTRUCTION

GENERAL PLAN CATEGORY: INDUSTRIAL TO RESIDENTIAL

ZONING: C2/PD

LOT SIZE: 281,188 (6.455 ACRES)

DENSITY: 44.92 UNITS PER ACRE (290 UNITS/ 6.455 ACRES)

PROPOSED USE: MIXED-USE

LOT COVERAGE: 32%

BUILDING HEIGHT: 75' MAX.

## VICINITY MAP



ESSEX PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1707 Fax: 650-494-1871

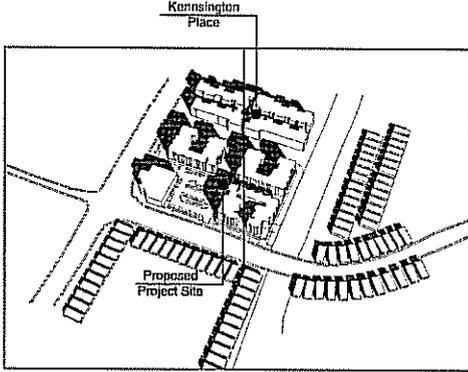
KTGY NO. 20060818

# TITLE SHEET 615 TASMAN DRIVE MIXED-USE Sunnyvale, California

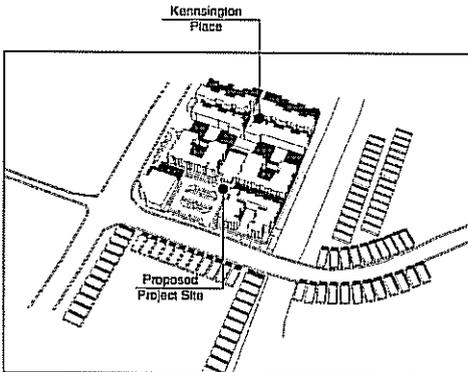
NO.	REVISIONS	DATE	BY
1	2ND PLANNING SUBMITTAL	06/20/07	ESSEX
2	3RD PLANNING SUBMITTAL	11/20/07	ESSEX



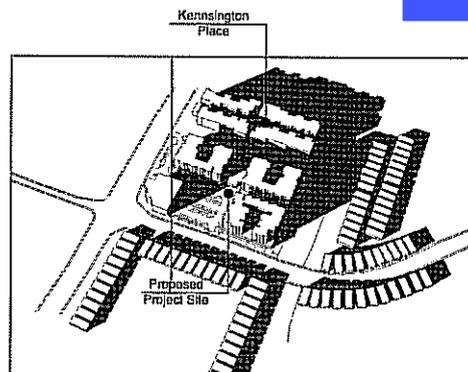
T-1  
03/08/07



Winter Solstice 9am



Winter Solstice 12pm

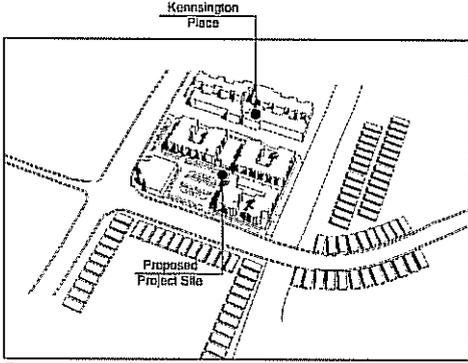


Winter Solstice 3pm

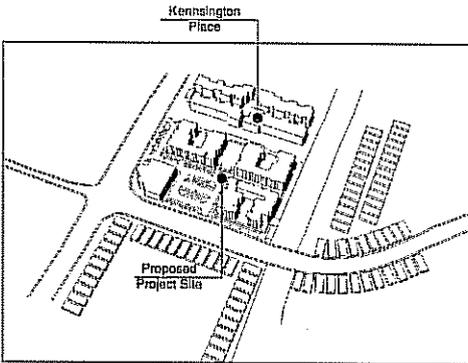


**WINTER SOLSTICE - December 21st**

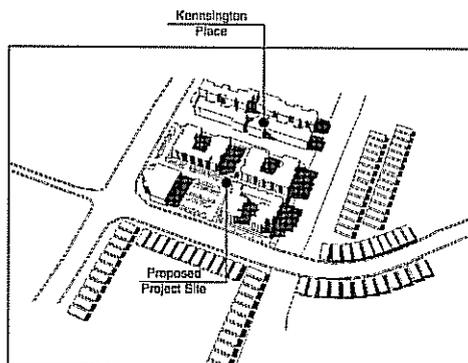
SUNRISE: 7:20 AM  
SUNSET: 4:50 PM



Summer Solstice 9am



Summer Solstice 12pm



Summer Solstice 3pm



**SUMMER SOLSTICE - June 21st**

SUNRISE: 5:45 AM  
SUNSET: 8:30 PM

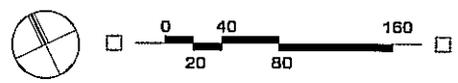
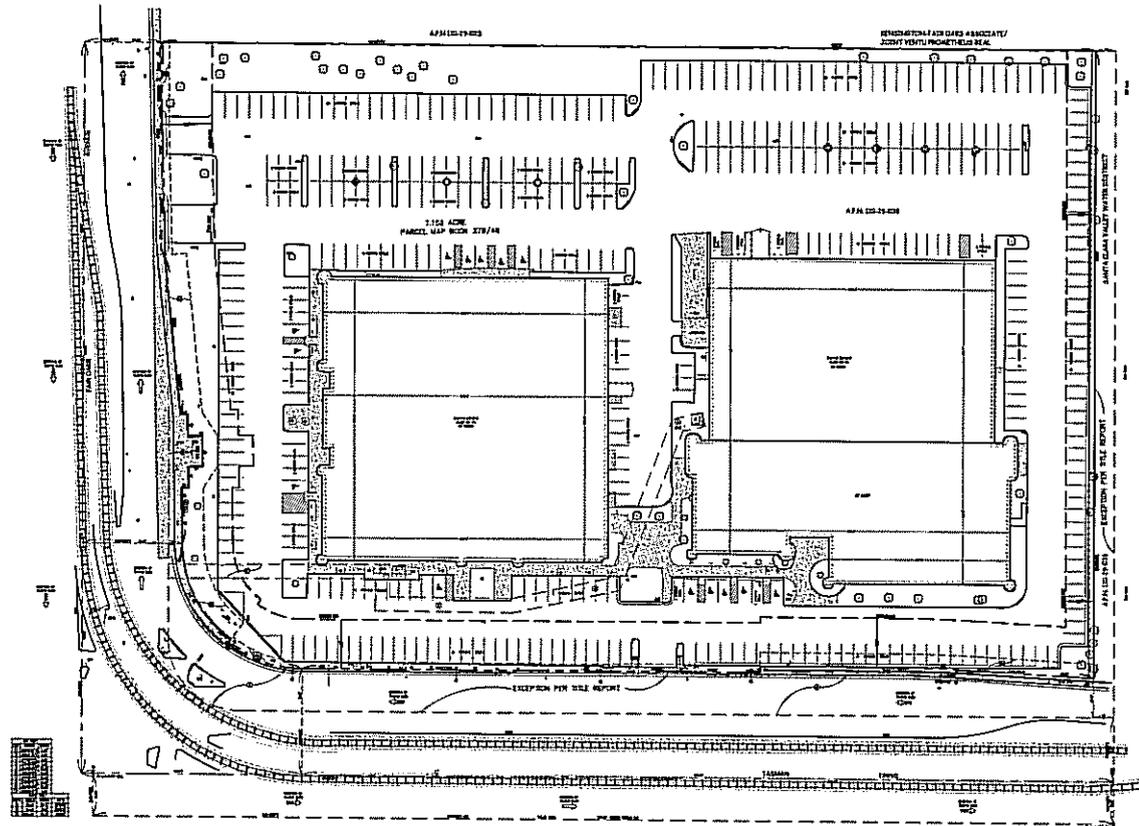
**ESSEX**  
PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-949-1707 Fax: 650-494-1671

**SUN & SHADE ANALYSIS**  
**615 TASMAN DRIVE MIXED-USE**  
Sunnyvale, California

#	REVISIONS	DATE	BY
1	3RD PLANNING SUBMITTAL	8/20/17	ESSEX
2	3RD PLANNING SUBMITTAL	10/24/17	ESSEX

**KTGY GROUP** INC.  
ARCHITECTURAL & INTERIOR DESIGN  
1700 STEVENS AVENUE  
SUNNYVALE, CALIFORNIA 94086  
Tel: 650-211-1143 Fax: 650-211-1144  
www.ktgy.com





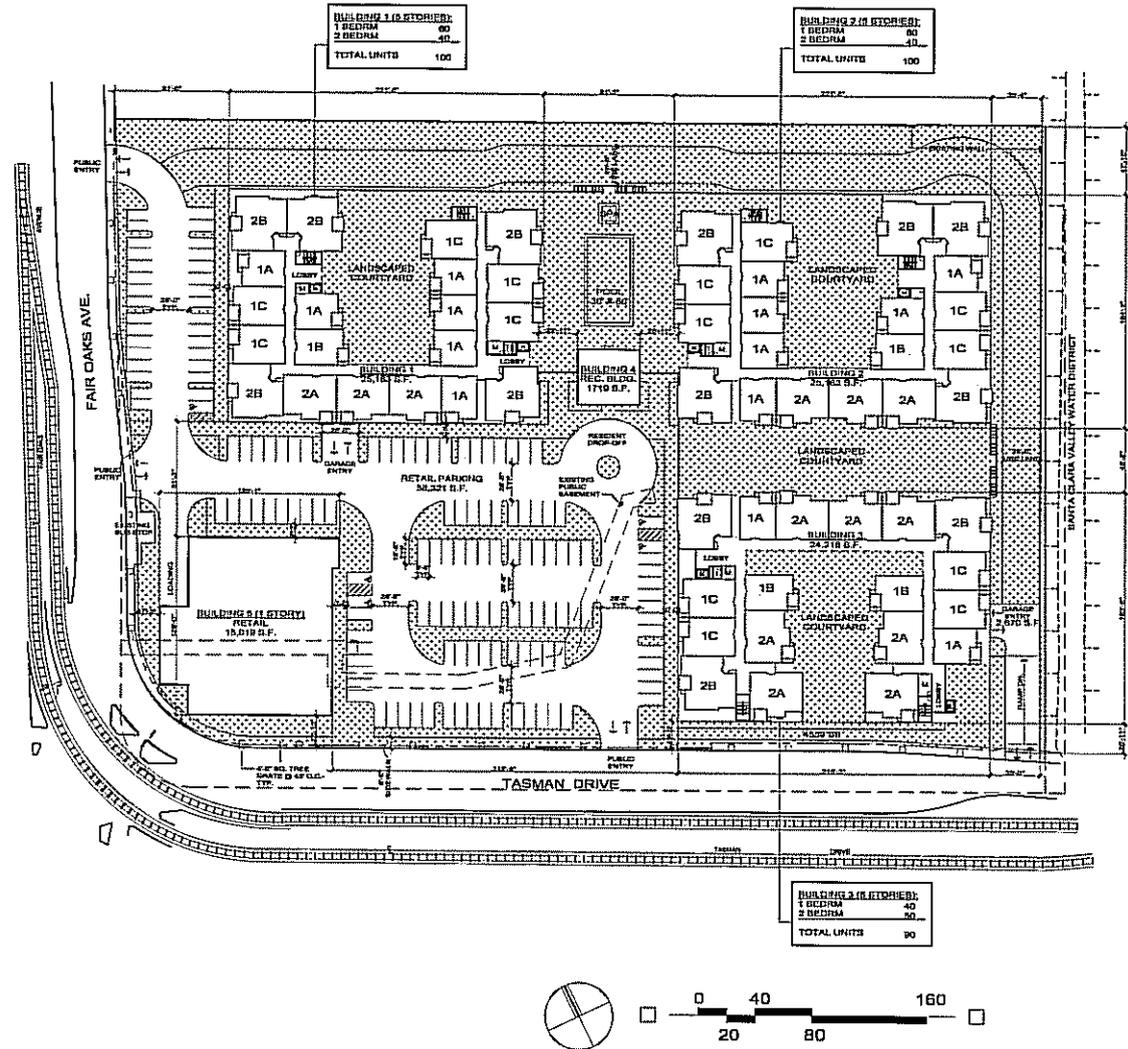
**ESSEX**  
 ESSEX PROPERTY TRUST, INC.  
 925 EAST MEADOW DRIVE  
 PALO ALTO, CALIFORNIA 94303  
 Tel: 650-849-1707 Fax: 650-494-1671  
 KTYG NO. 20060818

**EXISTING SITE PLAN**  
**615 TASMAN DRIVE MIXED-USE**  
 Sunnyvale, California

#	DESCRIPTION	DATE	BY
1	3RD PLANNING SUBMITTAL	8/20/07	ESSEX
2	3RD PLANNING SUBMITTAL	11/20/07	ESSEX



**SP-1**



**BUILDING 1 (4 STORIES)**

1 BEDRM	60
2 BEDRM	40
<b>TOTAL UNITS</b>	<b>100</b>

**BUILDING 2 (4 STORIES)**

1 BEDRM	60
2 BEDRM	40
<b>TOTAL UNITS</b>	<b>100</b>

**BUILDING 3 (4 STORIES)**

1 BEDRM	40
2 BEDRM	50
<b>TOTAL UNITS</b>	<b>90</b>

**PROJECT DATA TABLE**

<b>GENERAL PLAN CATEGORY</b>	INDUSTRIAL TO RESIDENTIAL MIXED-USE
<b>ZONING DISTRICT</b>	C2/ PD
<b>LOT SIZE (SQ. FT.)</b>	281,188 SQ. FT.
<b>GROSS FLOOR AREA (SQ. FT.)</b>	374,429 (RESIDENTIAL) 251,308 (RETAIL) 425,737 SQ. FT.
<b>NO. OF UNITS</b>	260 UNITS
<b>DENSITY (UNITS/ACRE)</b>	44.92 UNITS/ ACRE
<b>1 BEDROOM (894 SQ. FT. - 918 SQ. FT.)</b>	180 UNITS
<b>2 BEDROOM (1,043 SQ. FT. - 1,225 SQ. FT.)</b>	130 UNITS
<b>TOTAL</b>	260 UNITS
<b>LOCKABLE STORAGE</b>	300 CU. FT.
<b>LOT COVERAGE (%)</b>	32%
<b>FLOOR AREA RATIO (FAR)</b>	1.51
<b>NO. OF BUILDINGS ON-SITE</b>	5 BUILDINGS
<b>DISTANCE BETWEEN BUILDINGS (FT.)</b>	28 FT. TO 242 FT.
<b>BUILDING HEIGHT (FT.)</b>	75 FT.
<b>NO. OF STORIES</b>	6 STORIES
<b>FRONT SETBACK</b>	0 FT.
<b>LEFT SIDE SETBACK (FACING PROPERTY)</b>	0 FT.
<b>RIGHT SIDE SETBACK (FACING PROPERTY)</b>	30 FT.
<b>REAR SETBACK</b>	30 FT.
<b>LANDSCAPING (TOTAL SQ. FT.)</b>	128,817 SQ. FT.
<b>LANDSCAPING (SQ. FT./ UNIT)</b>	445 SQ. FT./ UNIT
<b>USABLE OPEN SPACE (SQ. FT.)</b>	130,801 SQ. FT.
<b>USABLE OPEN SPACE (SQ. FT./ UNIT)</b>	451 SQ. FT./ UNIT
<b>TOTAL NO. OF PARKING SPACES</b>	731 SPACES
<b>NO. OF STANDARDS</b>	686 SPACES
<b>NO. OF COMPACTS (% OF TOTAL)</b>	45 (6.1%)
<b>NO. OF HANDICAPS</b>	15
<b>NO. OF COVERED SPACES</b>	582 SPACES

**LEGEND**

LANDSCAPING  
 BUILDING  
 PAVING  
 LANDSCAPE AREA  
 LOT SIZE - 281,188 S.F. - 91,280 S.F. - 60,991 S.F. = 128,917 S.F.

<b>EST. SIZE AREA</b>	<b>281,188</b>
BUILDING 1	25,111
BUILDING 2	25,111
BUILDING 3	24,218
BUILDING 4	17,110
BUILDING 5	19,648
<b>RESIDENT COVERAGE TOTAL</b>	<b>109,298</b>
<b>PAVED PARKING AREA</b>	<b>60,991</b>
<b>LANDSCAPE AREA</b>	<b>128,817</b>

**ESSEX**  
PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1707 Fax: 650-494-1671  
KTGY NO. 20060816

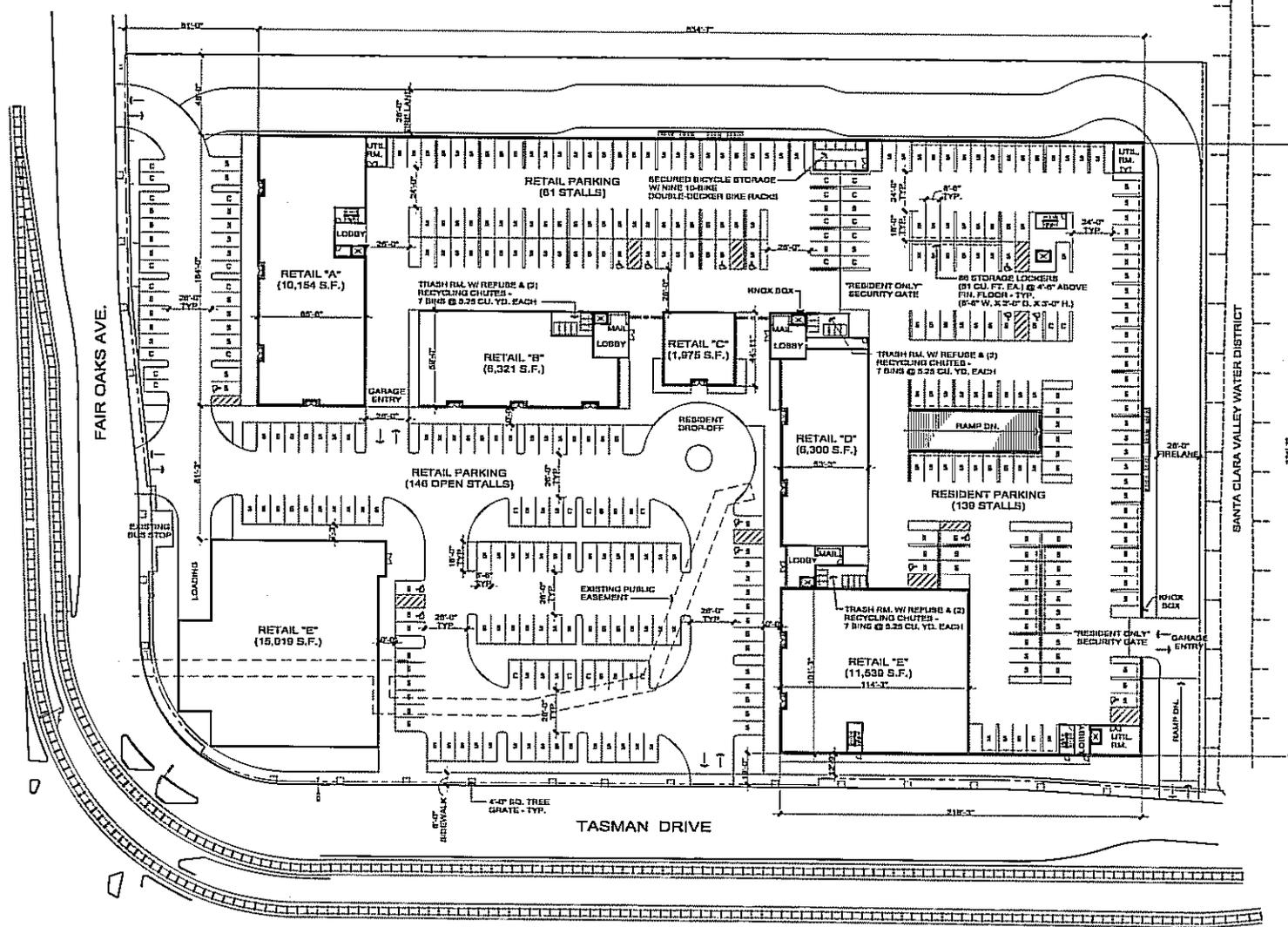
**PROPOSED SITE PLAN**  
**615 TASMAN DRIVE MIXED-USE**  
Sunnyvale, California

#	REVISION	DATE	BY
1	2ND PLANNING SUBMITTAL	02/07/16	ESSEX
2	3RD PLANNING SUBMITTAL	11/26/16	ESSEX

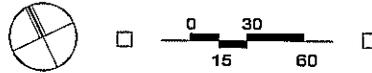
**KTGY GROUP**  
KENTWOOD TRUST, INC.  
17725 STEVENSON BOULEVARD  
SUNNYVALE, CALIFORNIA 94088  
Tel: 650-352-1100 Fax: 650-352-1100  
www.ktgy.com

**SP-2**  
03/08/17





GROUND LEVEL COMPOSITE PLAN

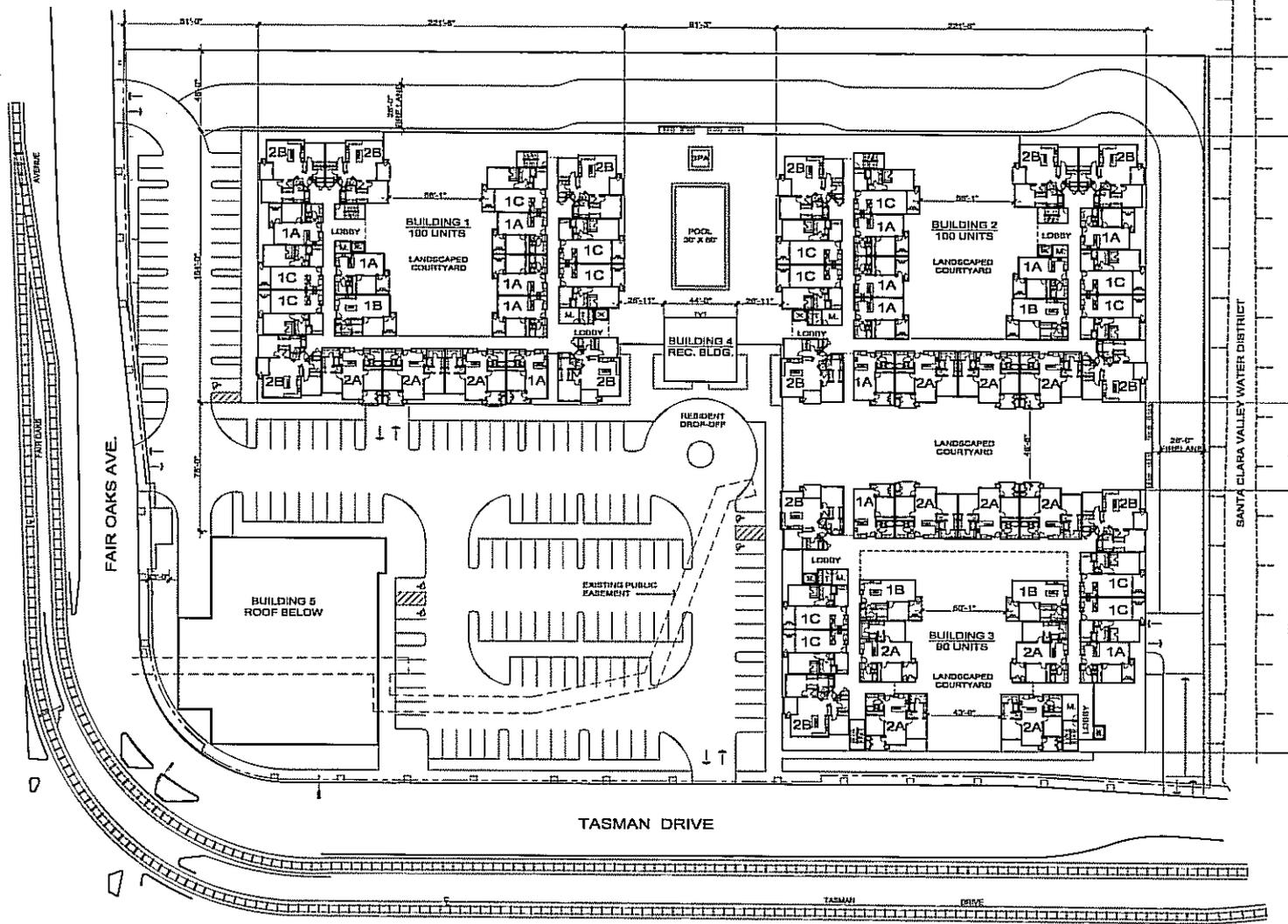


**COMPOSITE FLOOR PLAN**  
**615 TASMAN DRIVE MIXED-USE**  
Sunnyvale, California

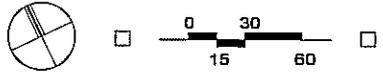
**ESSEX**  
PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1707 Fax: 650-494-1571

REVISION	DATE	BY
1	2ND PLANNING SUBMITTAL	8/20/07 ESKEN
2	3RD PLANNING SUBMITTAL	11/28/07 ESKEN





1ST FLOOR PLAN

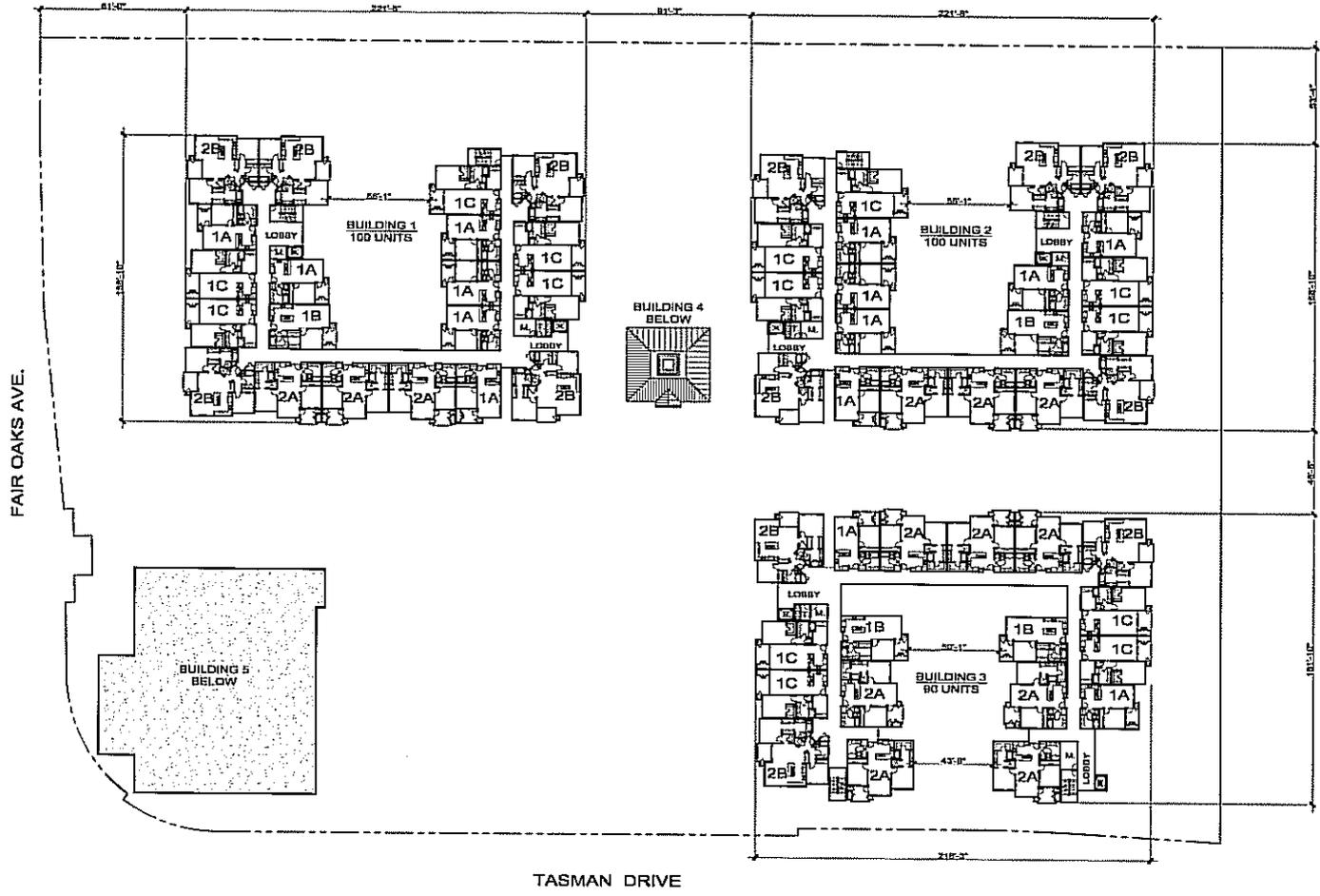


**COMPOSITE FLOOR PLAN**  
**615 TASMAN DRIVE MIXED-USE**  
Sunnyvale, California

**ESSEX**  
PROPERTY TRUST, INC.  
ESSEX PROPERTY TRUST, INC.  
825 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1707 Fax: 650-494-1671  
KTGY NO. 20090818

NO.	REVISIONS	DATE	BY
1	2ND PLANNING SUBMITTAL	02/20/07	ESSEX
2	3RD PLANNING SUBMITTAL	11/26/07	ESSEX

**KTGY GROUP, INC.**  
ARCHITECTS  
17073 CRYSTAL DRIVE  
IRVINE, CALIFORNIA 92614  
TEL: 949-215-7200 FAX: 949-215-7201  
www.ktgy.com



2ND FLOOR PLAN

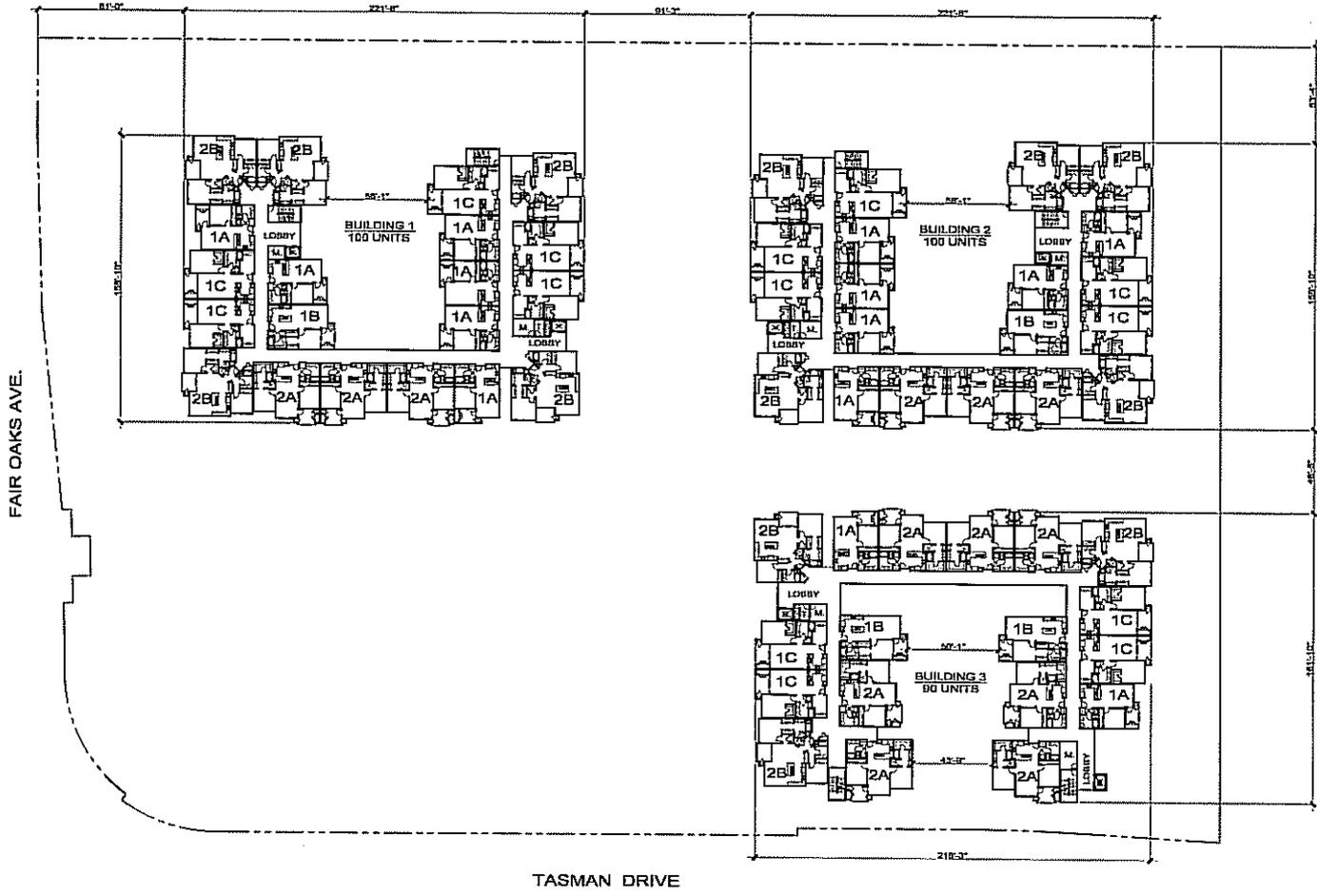


COMPOSITE FLOOR PLAN  
615 TASMAN DRIVE MIXED-USE  
Sunnyvale, California

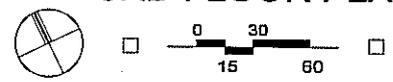
**ESSEX**  
PROPERTY TRUST, INC.  
ESSEX PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-945-1707 Fax: 650-994-1671  
KTGY NO. 20060816

#	REVISIONS	DATE	BY
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2	3RD PLANNING SUBMITTAL	11/28/07	ESSEX





3RD FLOOR PLAN

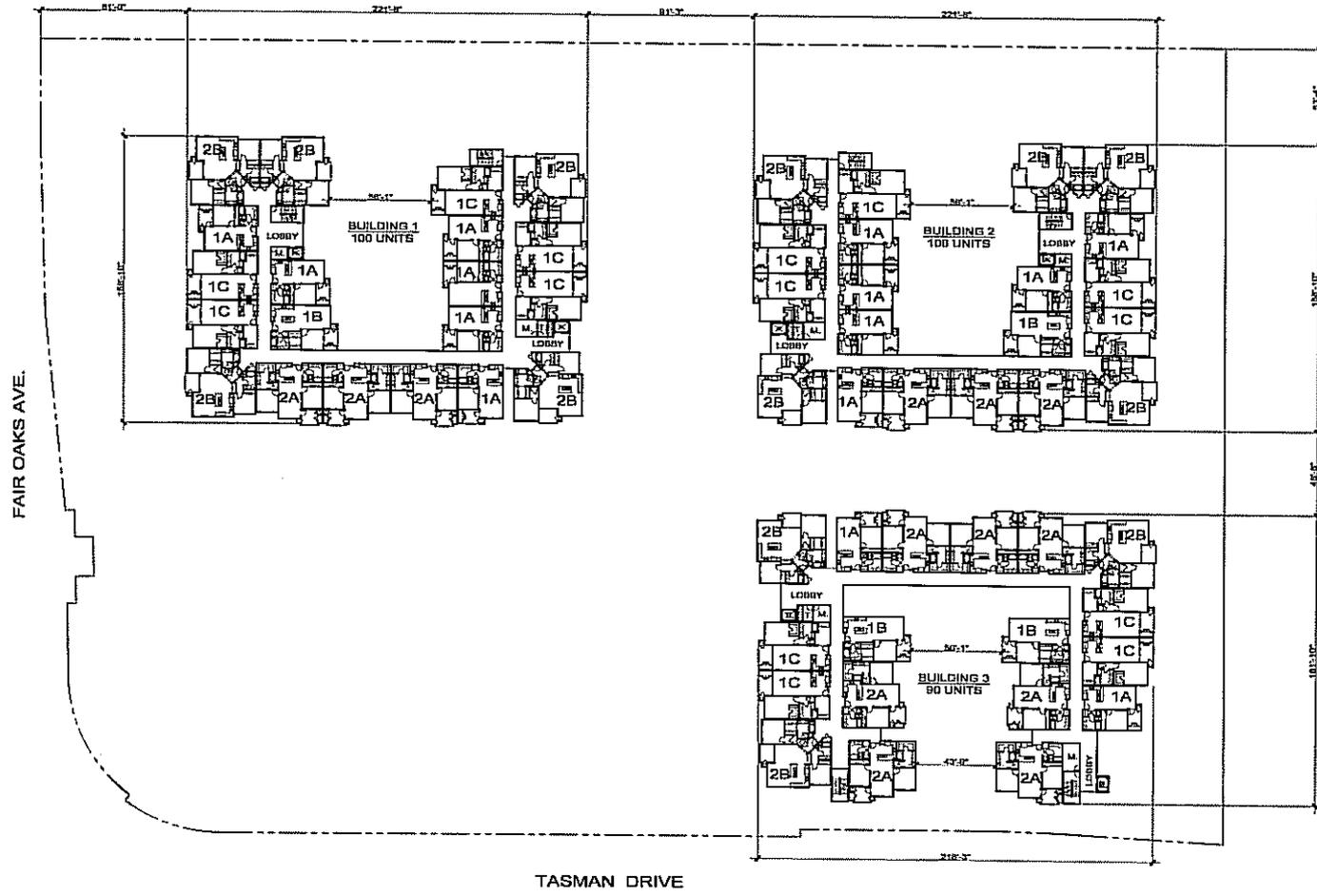


COMPOSITE FLOOR PLAN  
615 TASMAN DRIVE MIXED-USE  
Sunnyvale, California

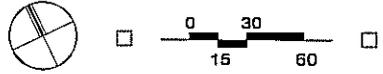
**ESSEX**  
PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1707 Fax: 650-494-1871

#	REVISIONS	DATE	BY
1	2ND PLANNING SUBMITTAL	8/20/07	ESSEX
2	3RD PLANNING SUBMITTAL	11/26/07	ESSEX





4TH FLOOR PLAN

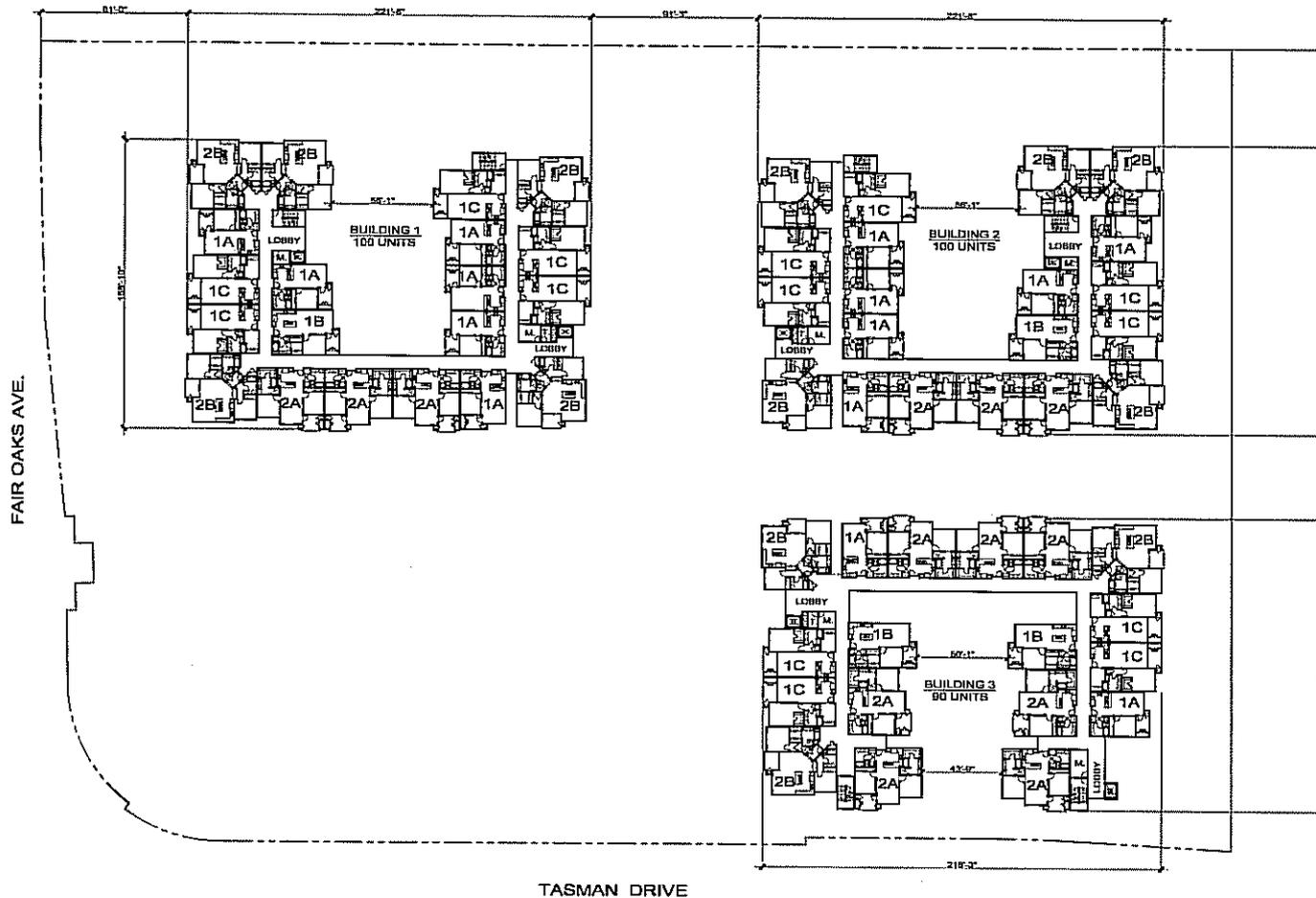


COMPOSITE FLOOR PLAN  
615 TASMAN DRIVE MIXED-USE  
Sunnyvale, California

REVISIONS	DATE	BY
1 3RD PLANNING SUBMITTAL		SCOTT FRICK
2 3RD PLANNING SUBMITTAL		FRANK BERNI



**ESSEX**  
PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1707 Fax: 650-494-1671



5TH FLOOR PLAN



COMPOSITE FLOOR PLAN  
615 TASMAN DRIVE MIXED-USE  
Sunnyvale, California

ESSEX

PROPERTY TRUST, INC.  
ESSEX PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-949-1707 Fax: 650-494-1671

KTGY NO. 20060810

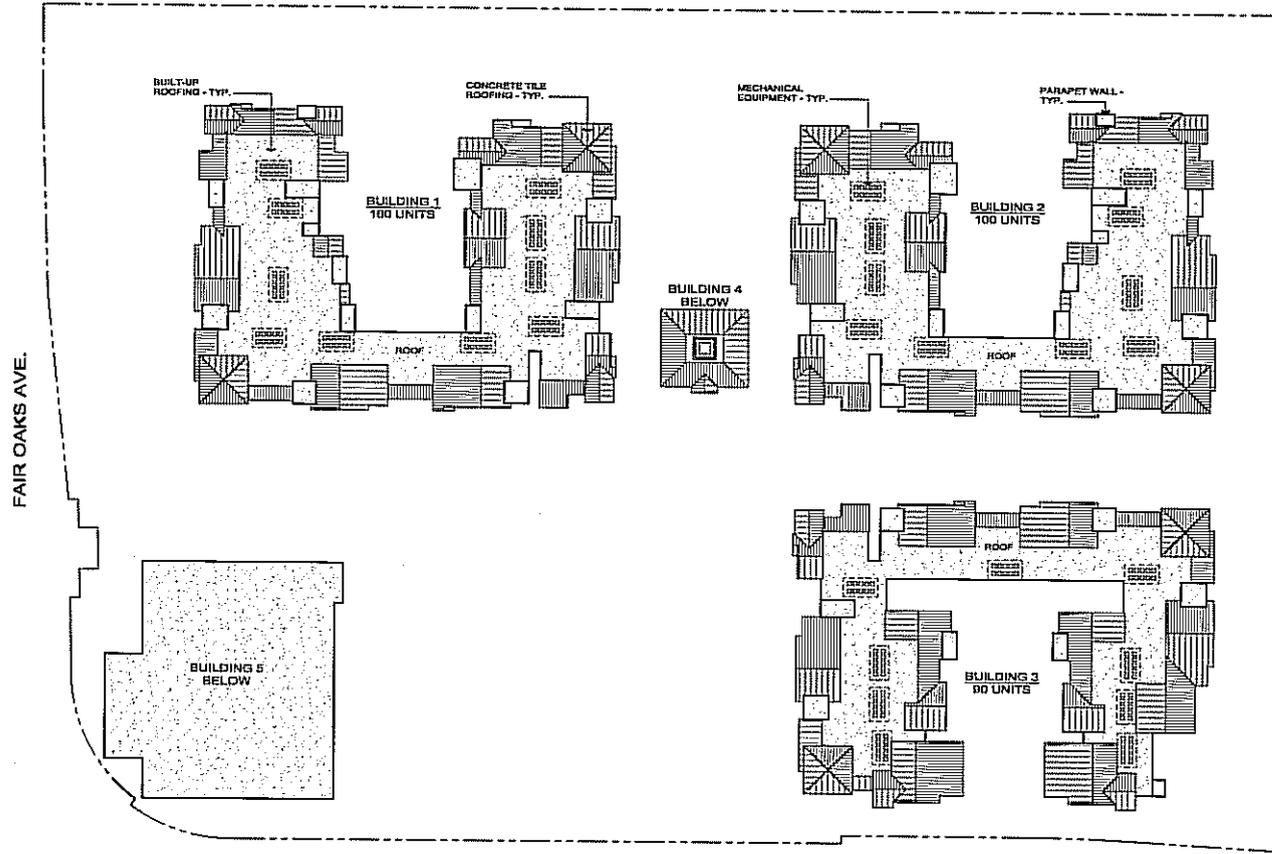
NO.	REVISIONS	DATE	BY
1	3RD PLANNING SUBMITTAL	02/20/07	ESSEX
2	3RD PLANNING SUBMITTAL	02/20/07	ESSEX
3	3RD PLANNING SUBMITTAL	02/20/07	ESSEX



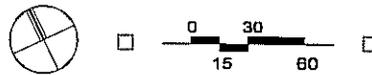
KTGY GROUP, INC.  
17221 CRYSTAL DRIVE  
FARMINGDALE, CALIFORNIA 94534  
Tel: (925) 938-7400 Fax: (925) 938-7400

A-7

03/08/07



ROOF PLAN

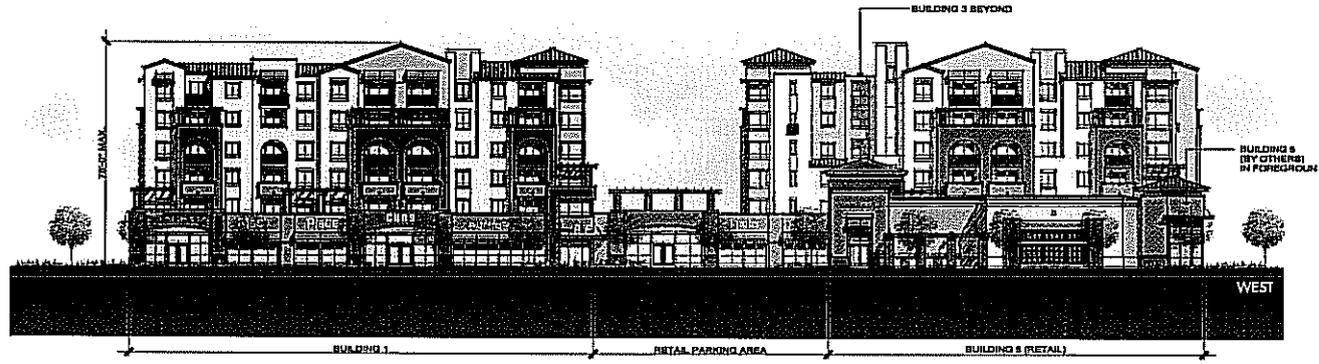


**ESSEX**  
PROPERTY TRUST, INC.  
ESSEX PROPERTY TRUST, INC.  
625 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1737 Fax: 650-494-1671

**COMPOSITE FLOOR PLAN**  
**615 TASMAN DRIVE MIXED-USE**  
Sunnyvale, California

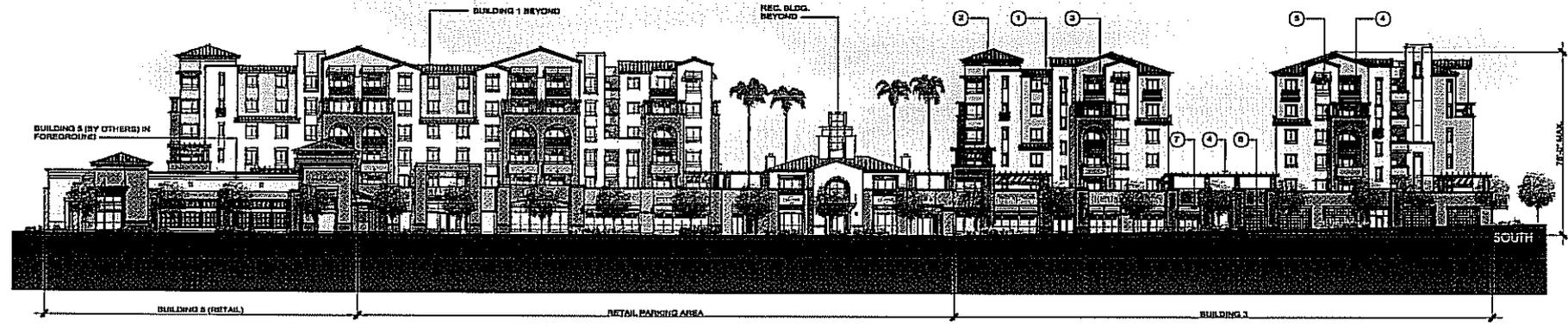
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1	2ND PLANNING SUBMITTAL		REGADY
2	3RD PLANNING SUBMITTAL		REGADY



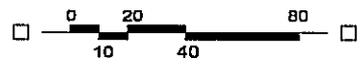


WEST ELEVATION (Fair Oaks Ave.)

- EXTERIOR MATERIALS**
1. WALLS: EXTERIOR PLASTER
  2. CONCRETE TILE ROOF
  3. RAILINGS: METAL RAILINGS AT DECKS
  4. TRELLIS - PAINTED W/ ACCENT COLOR
  5. WINDOW AWNING W/ ACCENT COLOR
  6. ARCHITECTURAL METAL GRILL AT GARAGE OPENINGS
  7. CANOPY OVERHANGS PROVIDED AT RETAIL SPACES



SOUTH ELEVATION (Tasman Drive)



**ESSEX**  
PROPERTY TRUST, INC.  
ESSEX PROPERTY TRUST, INC.  
825 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-849-1707 Fax: 650-484-1571  
KTGY NO. 20060018

**EXTERIOR ELEVATIONS**  
**615 TASMAN DRIVE MIXED-USE**  
Sunnyvale, California

REVISION	DATE	BY
1 2ND PLANNING SUBMITTAL	11/07/07	ESSEX
2 3RD PLANNING SUBMITTAL	11/07/07	ESSEX

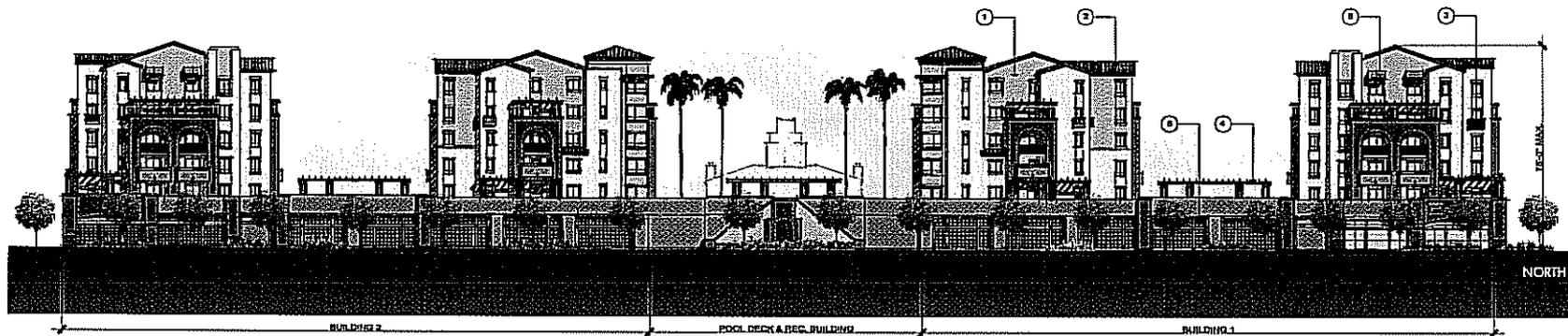




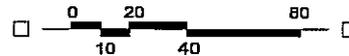
EAST ELEVATION

**EXTERIOR MATERIALS**

1. WALLS: EXTERIOR PLASTER
2. CONCRETE TILE ROOF
3. RAILINGS: METAL RAILINGS AT DECKS
4. TRELLIS - PAINTED W/ ACCENT COLOR
5. WINDOW AWNING W/ ACCENT COLOR
6. ARCHITECTURAL METAL GRILL AT GARAGE OPENINGS
7. CANOPY OVERHANGS PROVIDED AT RETAIL SPACES



NORTH ELEVATION



ESSEX PROPERTY TRUST, INC.  
825 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-949-1707 Fax: 650-484-1571  
KTGY NO. 20080818

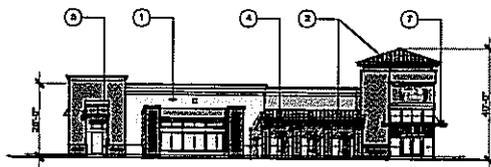
EXTERIOR ELEVATIONS  
615 TASMAN DRIVE MIXED-USE  
Sunnyvale, California

REV	REVISIONS	DATE	BY
1	2ND PLANNING SUBMITTAL	06/08/07	ESSEX
2	3RD PLANNING SUBMITTAL	11/20/07	ESSEX



A-10

03/08/07



BUILDING NO. 5  
EAST ELEVATION



BUILDING NO. 5  
SOUTH ELEVATION (Tasman Drive)

**EXTERIOR MATERIALS**

1. WALLS: EXTERIOR PLASTER
2. CONCRETE TILE ROOF
3. RAILINGS: METAL RAILINGS AT DECKS
4. TRELLIS - PAINTED W/ ACCENT COLOR
5. WINDOW AWNING W/ ACCENT COLOR
6. ARCHITECTURAL METAL GRILL
7. CANOPY OVERHANGS PROVIDED AT RETAIL SPACES



BUILDING NO. 5  
WEST ELEVATION (Fair Oaks Ave.)



BUILDING NO. 5  
NORTH ELEVATION



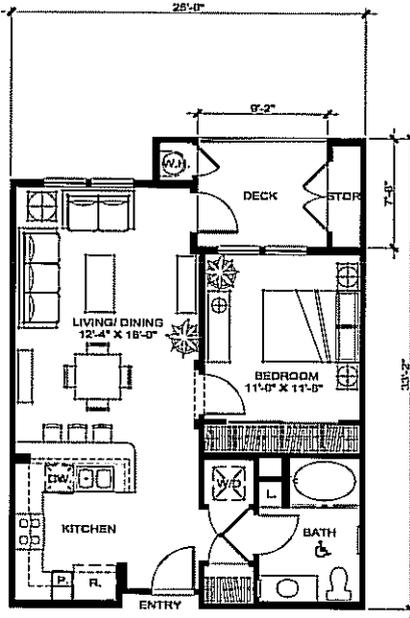
**ESSEX**

PROPERTY GROUP, INC.  
ESSEX PROPERTY TRUST, INC.  
925 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-949-1707 Fax: 650-494-1671  
KTGY NO. 20060918

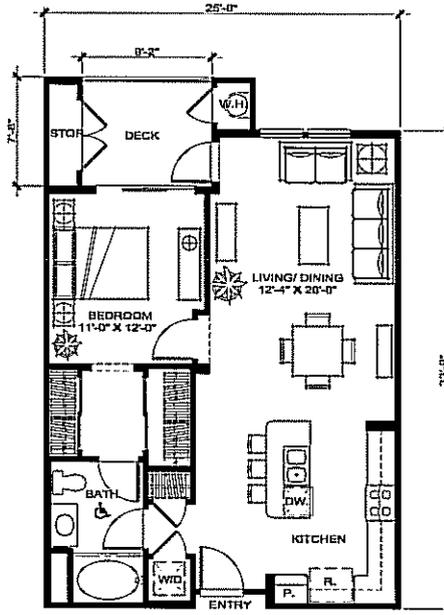
**EXTERIOR ELEVATIONS  
615 TASMAN DRIVE MIXED-USE  
Sunnyvale, California**

NO.	REVISIONS	DATE	BY
1	SUB PLANNING SUBMITTAL		SCOTT FERBER
2	SUB PLANNING SUBMITTAL		HEATHER FERBER

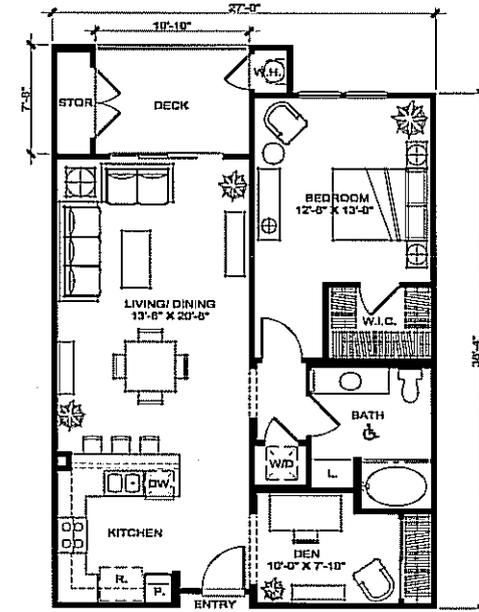




PLAN 1A - 1 BR/ 1 BATH  
LIVING AREA: 694 G.S.F.  
DECK AREA: 70 G.S.F.  
STORAGE: 129 CU. FT.



PLAN 1B - 1 BR/ 1 BATH  
LIVING AREA: 792 G.S.F.  
DECK AREA: 70 G.S.F.  
STORAGE: 129 CU. FT.



PLAN 1C - 1 BR/ 1 BATH  
LIVING AREA: 916 G.S.F.  
DECK AREA: 83 G.S.F.  
STORAGE: 150 CU. FT.



TYPICAL UNIT FLOOR PLANS  
615 TASMAN DRIVE MIXED-USE  
Sunnyvale, California

ESSEX

ESSEX PROPERTY TRUST, INC.  
825 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-649-1707 Fax: 650-404-1671

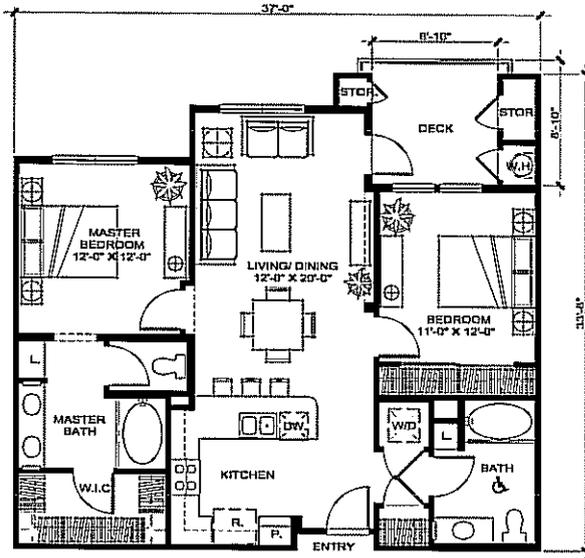
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2	3RD PLANNING SUBMITTAL	11/20/07	ESSEX

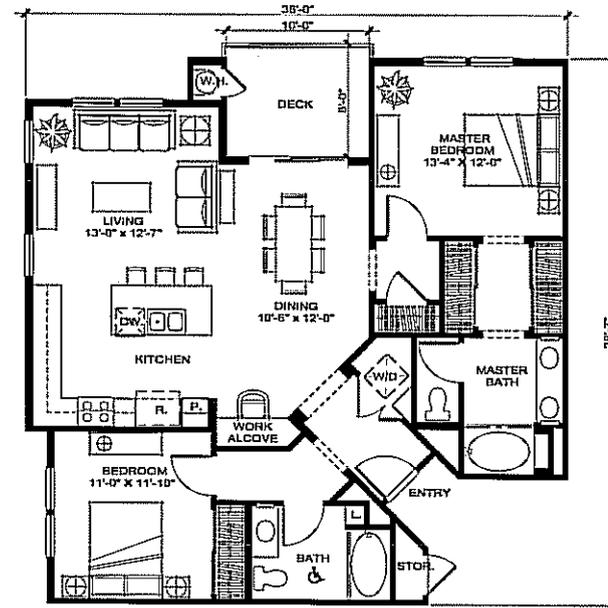


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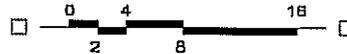
03/08/07



PLAN 2A - 2 BR/ 2 BATH  
LIVING AREA: 1,043 G.S.F.  
DECK AREA: 80 G.S.F.  
STORAGE: 124 CU. FT.



PLAN 2B - 2 BR/ 2 BATH  
LIVING AREA: 1,225 G.S.F.  
DECK AREA: 80 G.S.F.  
STORAGE: 81 CU. FT.



**ESSEX**

PROPERTY TRUST, INC.  
ESSEX PROPERTY TRUST, INC.  
926 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303  
Tel: 650-949-1707 Fax: 650-494-1671

KTGY NO. 20080818

**TYPICAL UNIT FLOOR PLANS**  
**615 TASMAN DRIVE MIXED-USE**  
Sunnyvale, California

REVISIONS	DATE	BY
1 2ND PLANNING SUBMITTAL	03/20/07	EDGEW
2 3RD PLANNING SUBMITTAL	11/02/07	EDGEW

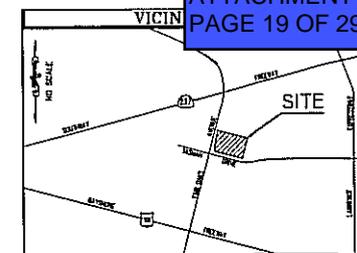
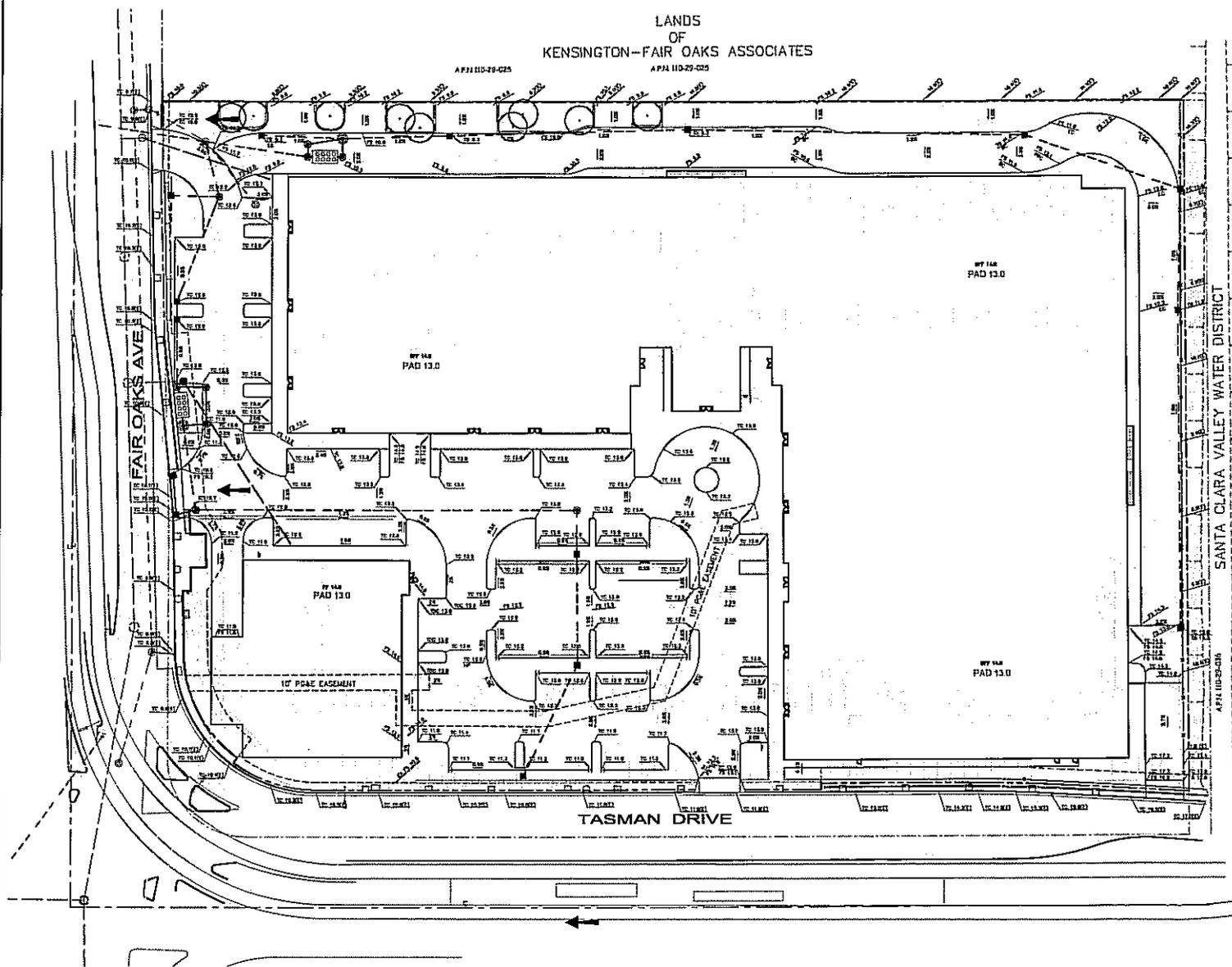


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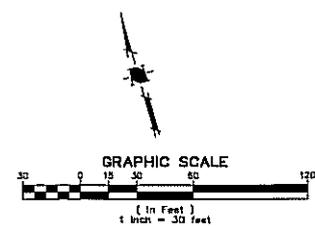
03/08/07

LANDS  
OF  
KENSINGTON-FAIR OAKS ASSOCIATES

AP# 110-29-025 AP# 110-29-025



- LEGEND**
- PROJECT BOUNDARY
  - EASEMENT
  - GARAGE FINISHED FLOOR ELEVATION
  - PAD ELEVATION
  - STREET SLOPE
  - OVERLAND RELEASE DIRECTION
  - STORM DRAIN MAIRICLE AND PIPE
  - SANITARY SEWER, MAIRICLE AND CLEANOUT
  - (C) STORM DRAIN MAIRICLE AND PIPE
  - (S) SANITARY SEWER, MAIRICLE
  - (E) FIRE HYDRANT
  - TOP OF CURB ELEVATION
  - FIELD SHOT
  - TOP OF DEPRESSION CURB
  - TOP OF GRADE ELEVATION
  - CATCH BASIN
  - INVERSION MAIRICLE (SEE DETAIL SHEET S-3)
  - STRENGTHEN VAULT (SEE DETAIL SHEET S-2)
  - CATCHBASIN FILTER (SEE DETAIL SHEET S-2)



BY	DATE	REVISIONS
AA	12/12/07	Per Planning Comments
TC	1/28/07	Per Client Comments
AA	1/15/07	Per Client Comments
AA	8/13/05	Per City Comments

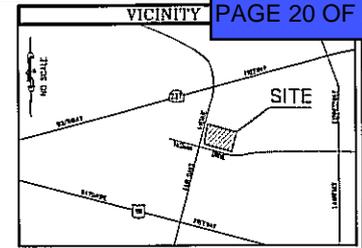
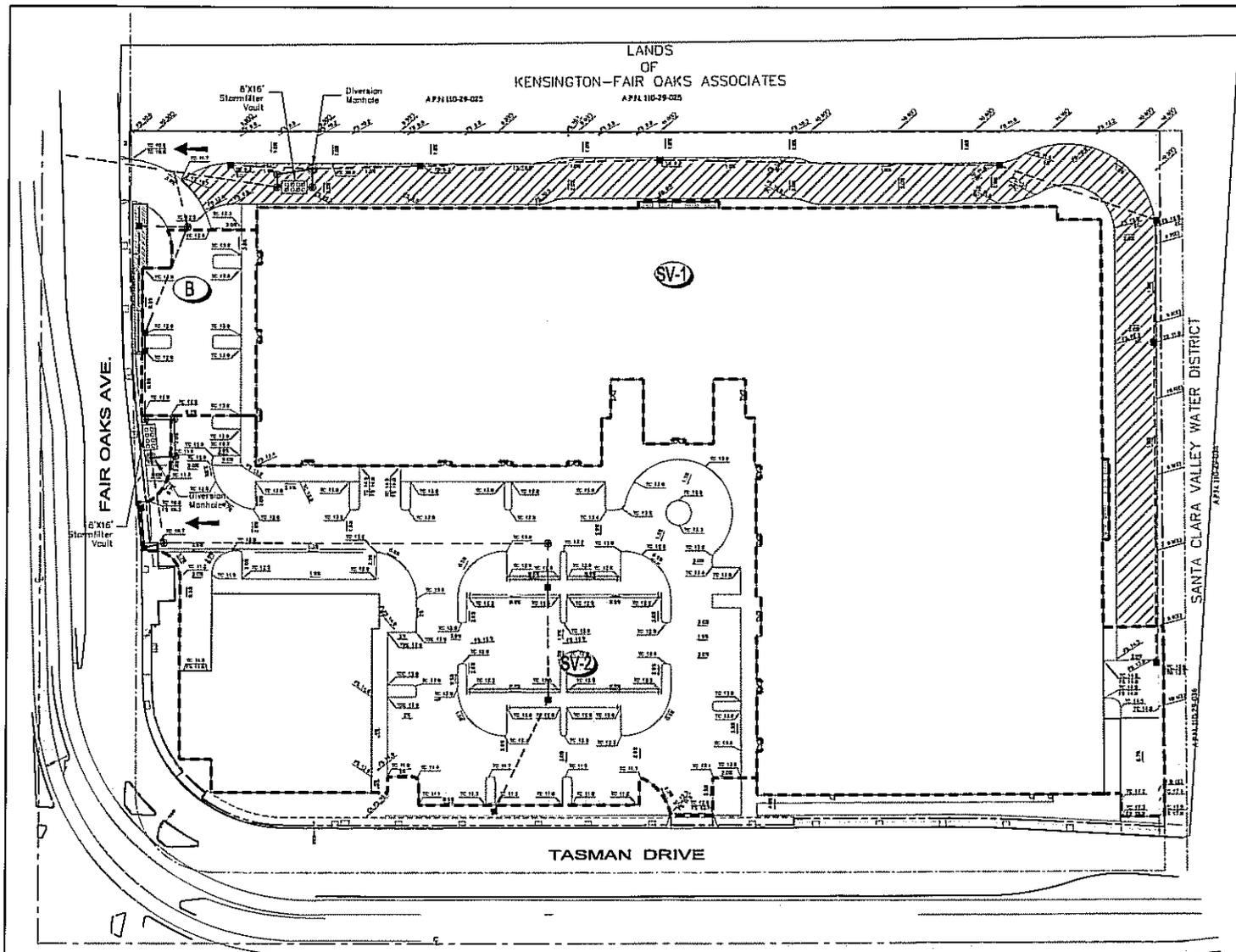
**ESSEX**  
PROPERTY TRUST, INC.

Date: 04.24.07  
Scale: 1"=30'  
Designed: AA  
Drawn: TC  
Checked: H.H.  
Proj. Eng: D.H.  
File: 3822002PSP

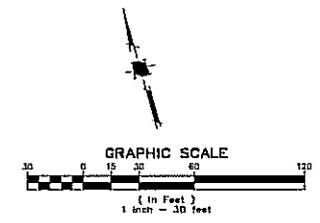
**HMH**  
ENGINEERS  
San Jose  
(408) 447-3200  
Elitoy  
(408) 448-0707  
www.hmh-engineers.com

SPECIAL DEVELOPMENT PERMIT  
Conceptual Grading and Utility Plan  
615 Tasman Drive, Sunnyvale, California

Sheet  
**C-1**  
JOB NUMBER  
**3620.00**



- LEGEND**
- PROJECT BOUNDARY
  - - - STORM DRAIN
  - - - EXISTING STORM DRAIN
  - - - ADJACENT PARCEL CENTER LINE
  - EXIST STORM MANHOLE
  - △ CURB INLET
  - ▲ HIGH POINT
  - ▼ LOW POINT
  - FL FLOW LINE
  - GFF GARAGE FRESH FLOOR
  - TC TOP OF CURB
  - TDC TOP OF DEPRESS CURB
  - TG TOP OF GRATE
  - STORM DRAIN MANHOLE
  - CATCH BASIN
  - ▲ SLOPE
  - ↑ OVERLAND RELEASE DIRECTION
  - FLOW DIRECTION
  - FLAT GRATE INLET
  - DIVERSION MANHOLE (SEE DETAIL SHEET 5-2)
  - STORMFILTER VAULT (SEE DETAIL SHEET 5-2)
  - BIDIRECTIONAL AREA (SEE DETAIL SHEET 5-3)
  - TURF BLOCK (SEE DETAIL SHEET 5-2)
  - DRAINAGE AREA BOUNDARY



△		
△	AA	1/24/08 Per Planning Comments
△	FC	1/28/07 Per Client Comments
△	AA	3/18/07 Per Client Comments
△	AA	8/31/07 Rev. City Comments
BT	DATE	REVISIONS

**ESSEX**  
PROPERTY TRUST INC.

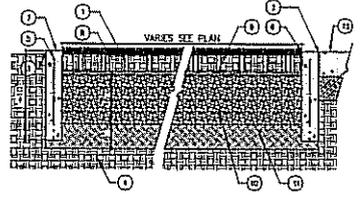
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Checked: MC  
Proj. Engr: URB  
File: 1620002000

**HMH** ENGINEERS  
San Jose  
(408) 447-3200  
Gilroy  
(408) 844-0707  
www.hmh-engineers.com

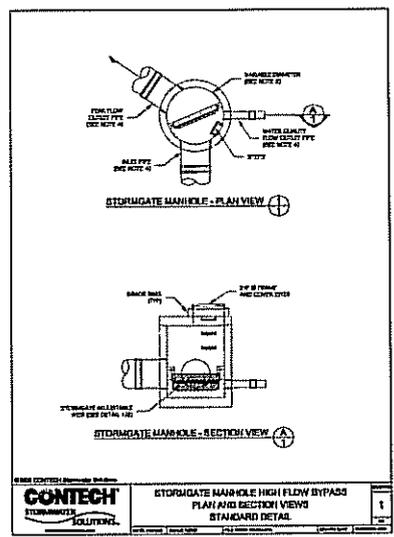
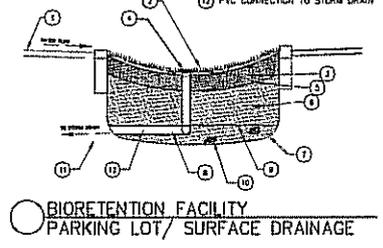
SPECIAL DEVELOPMENT PERMIT  
Stormwater Management Plan-Proposed Site Map  
615 Tasman Drive, Sunnyvale, California

Sheet  
**S-1**  
JOB NUMBER  
**3620.00**

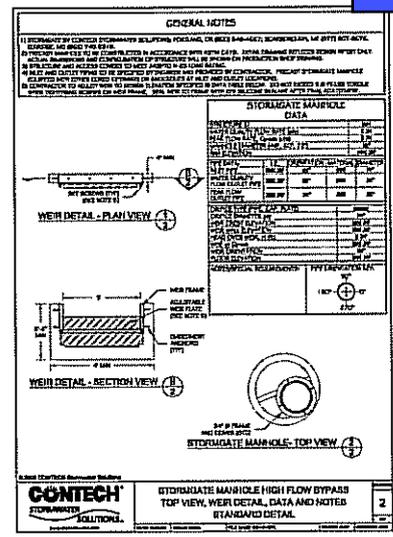
- 1 TURF BLOCK
- 2 EXPANSION JOINT
- 3 FRESH GRADE 1" BELOW FOR TURF 2" BELOW FOR SHRUB AREAS
- 4 NATIVE GRADE OR APPROVED COMPACTED SUBGRADE
- 5 TWO #3 REBAR @ 12" O.C. CONTIGUOUS
- 6 1/2" RADIUS EDGE
- 7 6" WIDE CONCRETE BAND
- 8 PLANTING BACKFILL
- 9 1" SAND BACKFILL
- 10 16" COMPACTED CLASS # AB ROCK
- 11 6" COMPACTED SUBGRADE TO SSS
- 12 CONCRETE PAVING



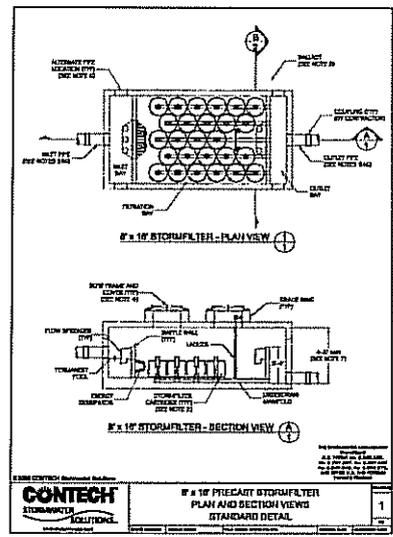
- NOTES:  
I. SEE PLANTING PLAN FOR PLANT TYPES.
- 1 PEDESTAL PAVERS OR OTHER HARDSCAPE (SEE LAYOUT AND MATERIAL PLANS, LANDSCAPE CONSTRUCTION DETAIL PLANS)
  - 2 VEGETATION (TURF OR GROUNDCOVER, TYP.)
  - 3 FILTER FABRIC
  - 4 SANDY LOAM MIX PERCOLATION RATE OF 3-10 IN. PER HOUR (MIN. 18" DEPTH)
  - 5 IMPERMEABLE LINER
  - 6 PERFORATED PVC UNDER-DRAIN
  - 7 FILTER FABRIC
  - 8 DRAIN ROCK
  - 9 NATIVE GRADE OR CERTIFIED COMPACTED SUBGRADE
  - 10 PVC CONNECTION TO STORM DRAIN



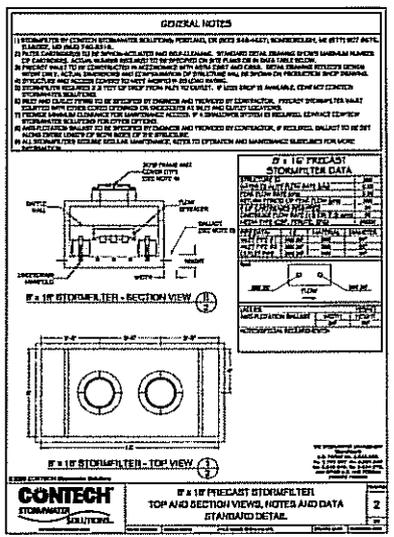
CONTECH STORMWATER SOLUTIONS  
STORMGATE MANHOLE HIGH FLOW BYPASS PLAN AND SECTION VIEWS STANDARD DETAIL  
1



CONTECH STORMWATER SOLUTIONS  
STORMGATE MANHOLE HIGH FLOW BYPASS TOP VIEW, WEIR DETAIL, DATA AND NOTES STANDARD DETAIL  
2



CONTECH STORMWATER SOLUTIONS  
PREFABRICATED STORMFILTER PLAN AND SECTION VIEWS STANDARD DETAIL  
1



CONTECH STORMWATER SOLUTIONS  
PREFABRICATED STORMFILTER TOP AND SECTION VIEWS, NOTES AND DATA STANDARD DETAIL  
2

AA	1/24/07	Per Planning Comments
FG	1/28/07	Per Client Comments
AA	3/18/07	Per Client Comments
AA	4/11/07	Per Client Comments
BY:	DATE:	REVISIONS:

ESSEX  
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Date: 04/24/07  
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Drawn: AW  
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Proj. Eng: DBB  
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Sheet S-2  
JOB NUMBER 3620.00

**StormFiltrer Unit Sizing**

The following steps for sizing the proposed StormFiltrer units are taken from the Product Design Manual, Version 3.3, released September 2005 by Stormwater Management, Inc. (product manufacturer). The Rational Method information contained in Step 1 is based on the methodology provided by the Santa Clara Valley Urban Runoff Pollution Prevention Program for calculating peak flow rates.

**Determining the Number of Cartridges for a Highly Impervious Site (>75% impervious)**

**Drainage Area SV-1 – Residential Podium Building**

**Step 1. Calculate the peak flow rate from the water quality storm ( $Q_{peak}$ ) for the site.**  
  
Use the Rational Method  $Q=CIA$  to solve for  $Q$ , given a rainfall intensity of 0.16 inches/hour, where  $Q$  = Flow (cubic feet/second),  $C$  = Runoff Coefficient,  $I$  = Rainfall Intensity (inches/hour), and  $A$  = Total Site Area (acres).

$C = .95$   
 $I = \text{Corrected Rainfall Intensity} = .16 \text{ in/hr}$   
 Correction Factor:  $13.2 \text{ in. MAP}_{obs} / 13.9 \text{ in. MAP}_{adj} = 0.95$   
 $.95 \times .17 \text{ in/hr} = .16 \text{ in/hr}$   
 $A = 139,504 \text{ sf} = 3.2 \text{ ac.}$

$Q = CIA = .95 \times .16 \text{ in/hr} \times 3.2 \text{ ac} = 0.49 \text{ cfs}$

**Step 2. Calculate the number of cartridges required to treat the peak water quality flow rate ( $N_{req}$ ) for the site.**

$N_{req} = Q_{peak} (449 \text{ gpm-cfs} / Q_{cartridge})$

Notes:

- Assume  $Q_{cartridge} = 15 \text{ gpm-cfs}$ , which is the maximum flow rate that an individual cartridge can treat.
- If the number of cartridges is not a whole number, round the number of cartridges up to the next whole number.

$N_{req} = (.49 \text{ cfs}) \times (449 \text{ gpm-cfs} / 15 \text{ gpm-cfs}) = 14.7 = 15 \text{ cartridges}$

**Drainage Area SV-2 – Commercial Building, Surface Parking**

**Step 1. Calculate the peak flow rate from the water quality storm ( $Q_{peak}$ ) for the site.**

Use the Rational Method  $Q=CIA$  to solve for  $Q$ , given a rainfall intensity of 0.16 inches/hour, where  $Q$  = Flow (cubic feet/second),  $C$  = Runoff Coefficient,  $I$  = Rainfall Intensity (inches/hour), and  $A$  = Total Site Area (acres).

$C = .95$   
 $I = \text{Corrected Rainfall Intensity} = .16 \text{ in/hr}$   
 $A = 62,176 \text{ sf} = 1.69 \text{ ac.}$

$Q = CIA = .95 \times .16 \text{ in/hr} \times 1.69 \text{ ac} = 0.26 \text{ cfs}$

**Step 2. Calculate the number of cartridges required to treat the peak water quality flow rate ( $N_{req}$ ) for the site.**

$N_{req} = Q_{peak} (449 \text{ gpm-cfs} / Q_{cartridge})$

Notes:

- Assume  $Q_{cartridge} = 15 \text{ gpm-cfs}$ , which is the maximum flow rate that an individual cartridge can treat.
- If the number of cartridges is not a whole number, round the number of cartridges up to the next whole number.

$N_{req} = (.26 \text{ cfs}) \times (449 \text{ gpm-cfs} / 15 \text{ gpm-cfs}) = 8.7 = 9 \text{ cartridges}$

**Bioretention Area Sizing**

The sizing for the proposed bioretention area was calculated based on the volume hydraulic design method contained in Provision C.3 of the NPDES Permit (the volume of annual runoff required to achieve 80% or more capture, determined in accordance with the methodology set forth in Appendix D of the *California Stormwater Best Management Practices Handbook, (1993)*, using local rainfall data).

**Area B**

Assumptions: 8,050 ft<sup>2</sup> total area draining to bioretention cell

**Step 1 Determine drainage area for the BMP**

Total Drainage Area to BMP  
= 8,050 ft<sup>2</sup>

**Step 2 Determine percent imperviousness of the drainage area**

95% (estimated)

**Step 3 Determine Mean Annual Precipitation**

= 13.2 in.

**Step 4 Identify rain gage closest to the site**

SJ Airport MAP<sub>adj</sub> = 13.9 in.

**Step 5 Determine rain gage correction factor**

= 13.2 in. / 13.9 in.  
= .95

**Step 6 Identify representative soil type for drainage area**

clay

**Step 7 Determine average slope for the drainage area**

= .01 or 1%

**Step 8 Determine unit basin storage from sizing curves (% imperviousness vs. soil type)**

= .58 in.

**Step 9 Size the BMP**

Basin volume = rain gage correction factor x unit basin storage volume x drainage area  
= .95 x .58 in. x 8,050 ft<sup>2</sup> x (1 ft/12 in.)  
= 370 ft<sup>3</sup>

**Summary Required storage volume = 370 ft<sup>3</sup>**

AA	1/31/07	Rev Planning Comments
FC	1/26/07	Rev Client Comments
AA	1/19/07	Rev Client Comments
AA	1/31/07	Rev City Comments
DT	DATE	REVISIONS



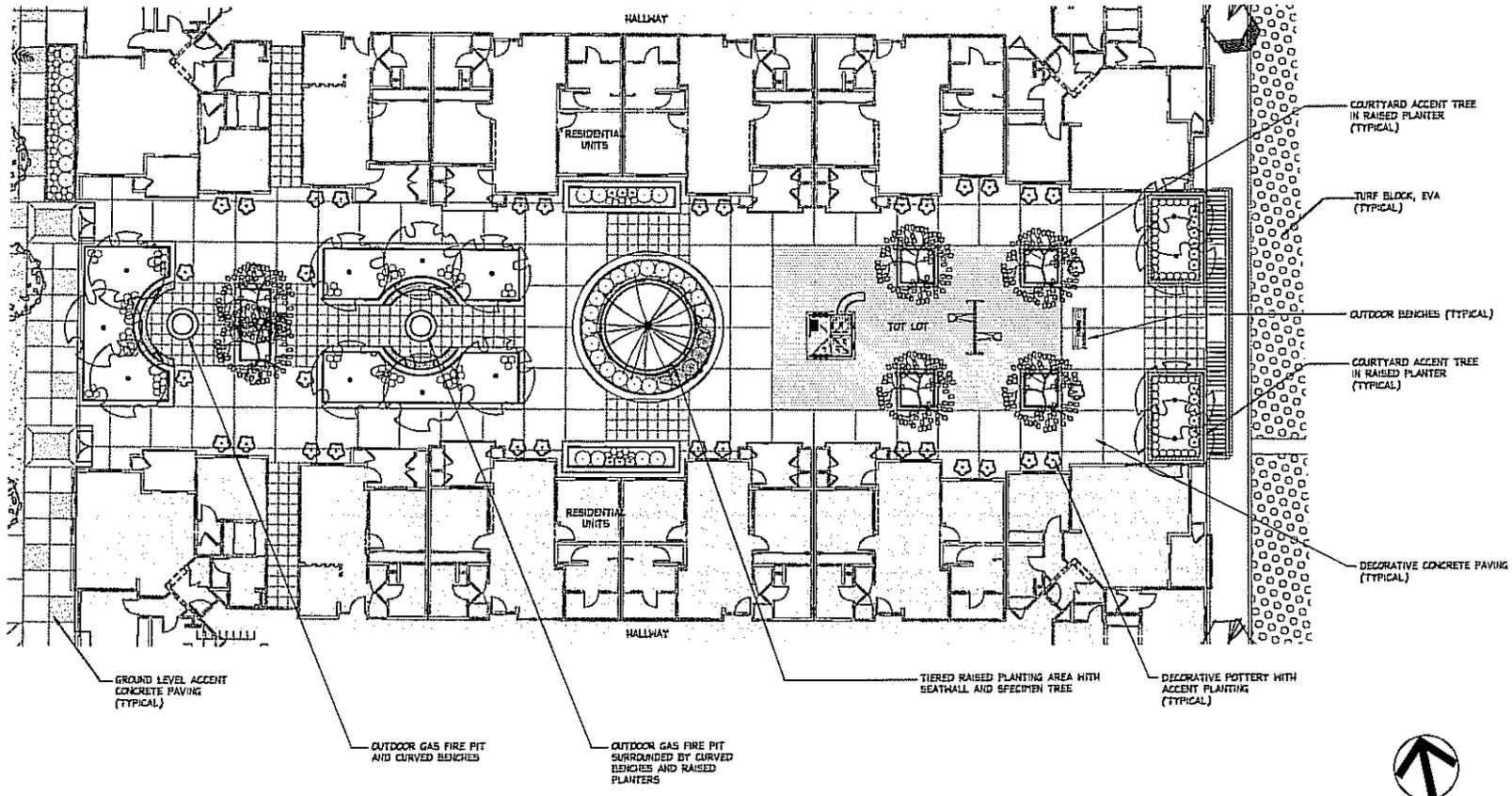
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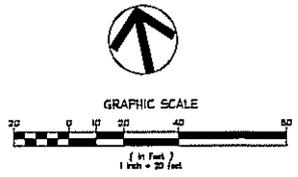
SPECIAL DEVELOPMENT PERMIT  
 Stormwater Management Plan-Details, Sizing Calculations  
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Sheet <b>S-3</b>
JOB NUMBER <b>3620.00</b>





COURTYARD ENLARGEMENT



△		
△	AA	1/24/08 Per Planning Comments
△	FC	1/28/07 Per Client Comments
△	AA	1/18/07 Per Client Comments
△	AA	1/11/07 Per City Comments
△	BT	DATE REVISIONS

**ESSEX**  
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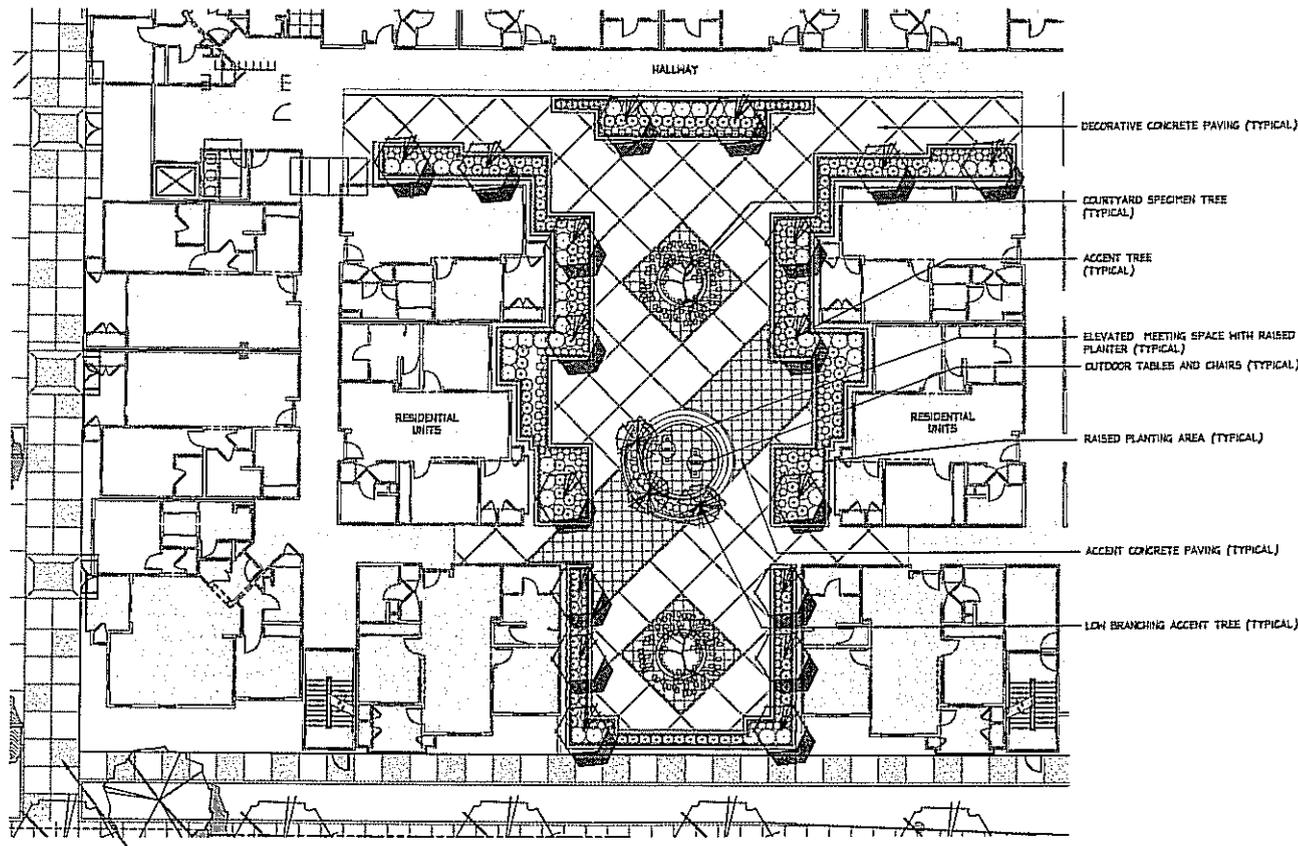
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Proj. Engr:	5/11/07
File:	200704.DWG

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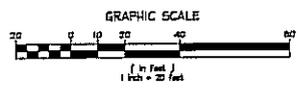
SHEET	L-2
JOB NUMBER	3620.00



- DECORATIVE CONCRETE PAVING (TYPICAL)
- COURTYARD SPECIMEN TREE (TYPICAL)
- ACCENT TREE (TYPICAL)
- ELEVATED MEETING SPACE WITH RAISED PLANTER (TYPICAL)
- OUTDOOR TABLES AND CHAIRS (TYPICAL)
- RAISED PLANTING AREA (TYPICAL)
- ACCENT CONCRETE PAVING (TYPICAL)
- LOW BRANCHING ACCENT TREE (TYPICAL)

GROUND LEVEL ACCENT CONCRETE PAVING (TYPICAL)

COFFEE-COURT COURTYARD ENLARGEMENT



△		
△	AA	12/10/07 Per Planning Comments
△	JC	11/28/07 Per Client Comments
△	AA	11/16/07 Per Client Comments
△	AA	10/11/07 Per Client Comments
BY	DATE	REVISIONS

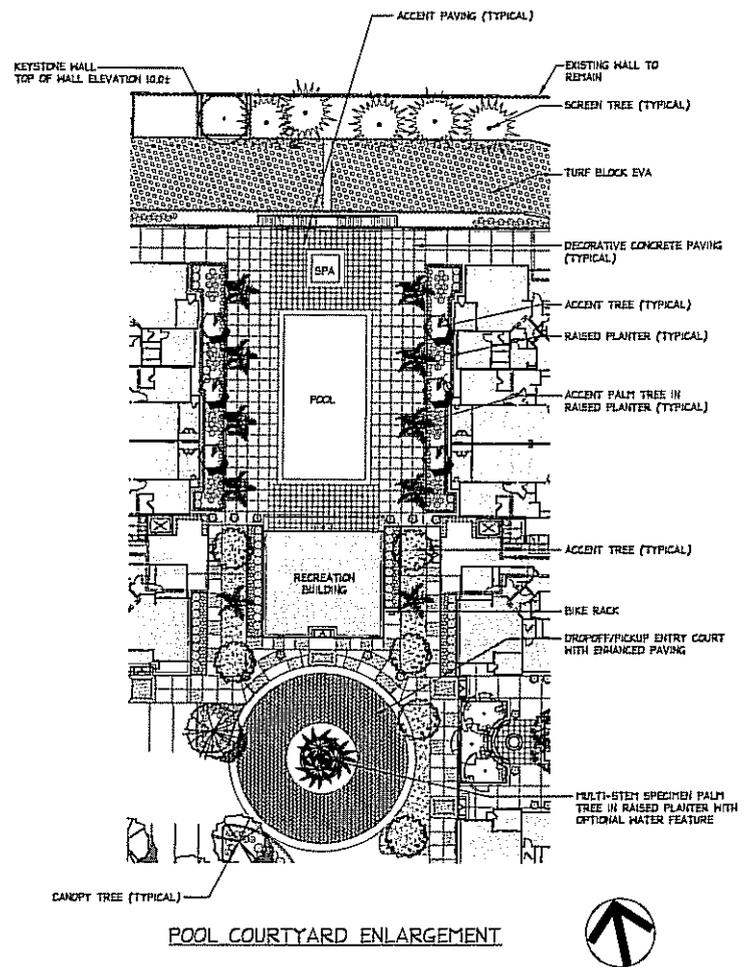
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 FNo: M0000000

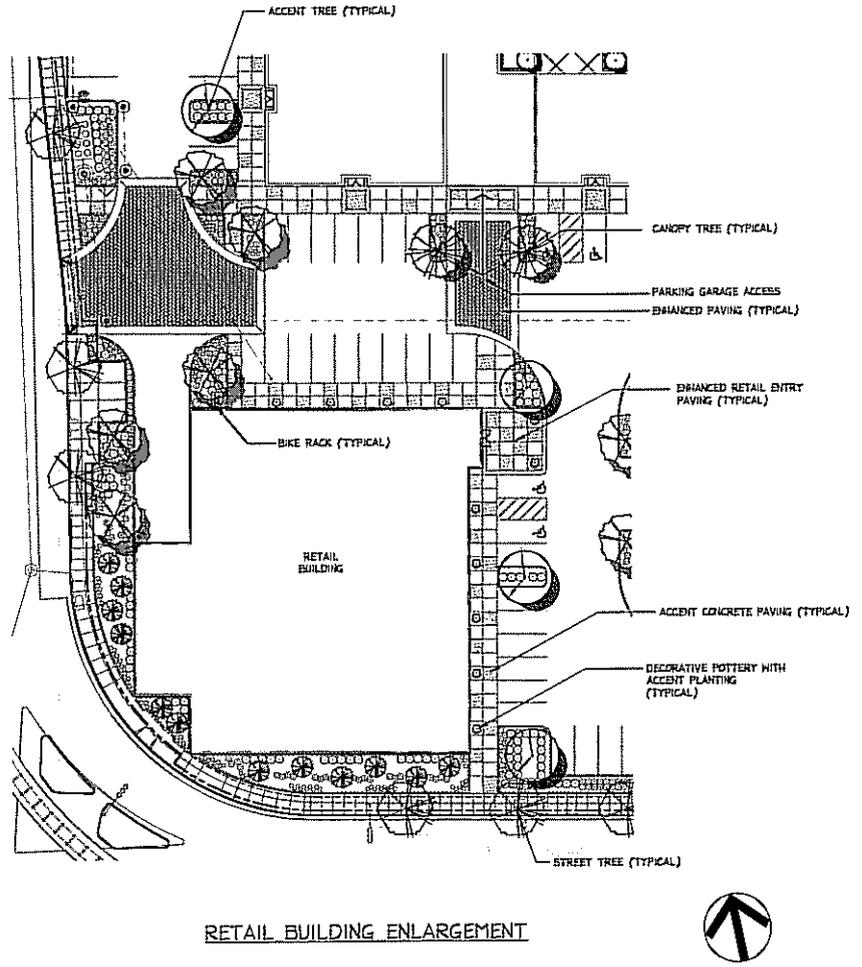
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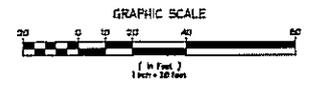
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3620.00



POOL COURTYARD ENLARGEMENT



RETAIL BUILDING ENLARGEMENT



Δ		
Δ		
Δ	AA	12/12/07 Per Planning Comments
Δ	FC	1/28/07 Per Client Comments
Δ	AA	1/18/07 Per Client Comments
Δ	AA	1/23/07 Per City Comments
BY	DATE	REVISIONS

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Proj. Engr: JHJ  
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L-4  
JOB NUMBER  
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**Hunico**  
SUPPLY, LLC

SELECT DESIRED INSTALLATION OPTION  
 1. FLANGE HEIGHT (FH)  
 2. FROM GROUND POINT (FG)

SELECT DESIRED FINISH AND COLOR  
 1. POLYMERIZED (POLY)  
 2. CONCRETE PLATED (CP)  
 3. STEEL  
 4. POLYURETHANE  
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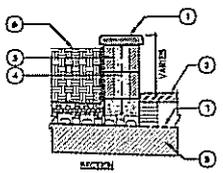
16" (1425mm)  
 18" CENTER TO CENTER (4575mm)  
 48" (1219mm)  
 BR PLUS 5  
 60 LBS

NOTES:  
 1. INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.  
 2. DO NOT SCALE DRAWINGS.  
 3. DIMENSIONS APPLY FOR ALL RACKS. OPTIONAL HEIGHT OF 48" (1219mm) IS AVAILABLE.  
 4. THE TIE-BARS INDICATE THE NUMBER OF TIE-BARS EACH BIKE RACK REQUIRES.  
 5. REFER TO DRAWING NUMBER DW-024 FOR DETAILS.  
 6. CONTRACTOR NOTE FOR PROJECT AND PURCHASING INFORMATION VISIT  
[WWW.HUNICO.COM](http://WWW.HUNICO.COM) REFERENCE NUMBER DW-024

**D BR PLUS SERIES BIKE RACKS**  
 NOT TO SCALE (6 BIKE CAPACITY)  
 PROTECTED BY COPYRIGHT - DW-024 02/26/03 [www.hunico.com](http://www.hunico.com)

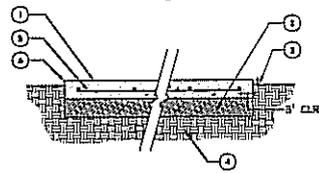
UNITED SUPPLY, LLC  
 P.O. BOX 1036  
 PORTLAND, OREGON 97206-0368  
 1-800-447-4474  
 PHONE (503) 754-8700  
 FAX (503) 274-0265  
[WWW.UNITEDSUPPLY.COM](http://WWW.UNITEDSUPPLY.COM)

- 1 STRUCTURAL SLAB
- 2 PLANTING AREA AT REAR OF HALL
- 3 WATER PROOF MEMBRANE BY OTHERS

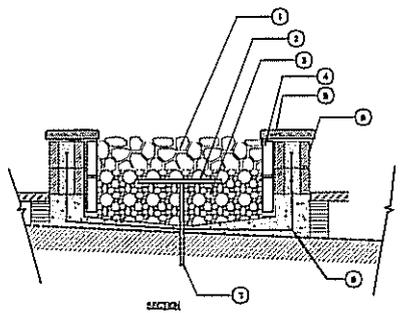


**C PLANTER WALL**  
 SCALE 1/4"=1'-0"

- 1 CSPU CAP
- 2 CONCRETE PAVERS ON TERRA SYSTEM OR MORTAR. SEE PLAN FOR LOCATIONS. SEE DETAIL.
- 3 6"x6"x16" OR 6"x8"x16" SINGLESIDE SPLITFACE CMU. SPLITFACE TO FACE EXPOSED SIDES. GROUT ALL CELLS SOLID.
- 4 #3 REBAR @ 18" O.C. EACH WAY TIE IN TO CURB OR ANCHOR



**A CONCRETE PAVING (PEDESTRIAN)**  
 SCALE 1/4"=1'-0"

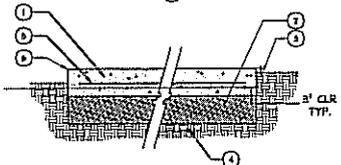


**E FIRE PIT**  
 NOT TO SCALE

- 1 2" TO 4" DECO ROCK
- 2 16" ROUND GAS FIRE RING
- 3 PEA GRAVEL
- 4 COMMON FIRE BRICK
- 5 CSPU CAP TO MATCH PLANTERS
- 6 NO. 4 REBAR, CONTINUOUS
- 7 3/4" GAS LINE PER LOCAL CODES
- 8 NO. 4 REBAR @ 16" O.C.

NOTES:  
 1. SEE MATERIALS LIST FOR COLOR TYPE AND FINISH.

- 1 4" THICK CONCRETE PAVING
- 2 4" THICK COMPACTED (PS) CLASS 2 AGGREGATE BASE PER GEOTECHNICAL REPORT
- 3 FINISH GRADE 1" BELOW FOR TURF 2" BELOW FOR SHRUBS AREAS
- 4 COMPACTED SUBGRADE
- 5 #3 REBAR @ 18" O.C. EACHWAY
- 6 1/2" RADIUS EDGE



**B CONCRETE PAVING (VEHICULAR)**  
 SCALE 1/4"=1'-0"

AA	1/24/07	Per Planning Comments
AA	1/28/07	Per Client Comments
AA	1/18/07	Per Client Comments
AA	1/11/07	Per City Comments
BY	DATE	REVISIONS

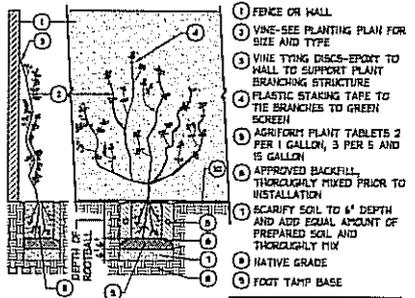
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Date: 04.24.07  
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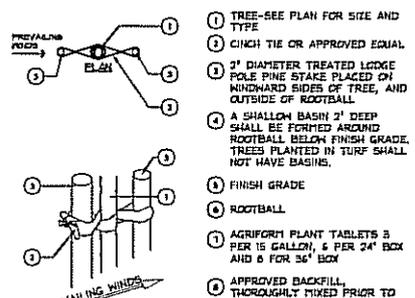
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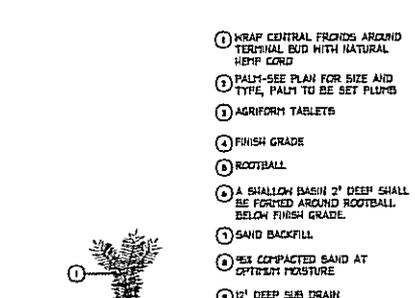
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 3620.00



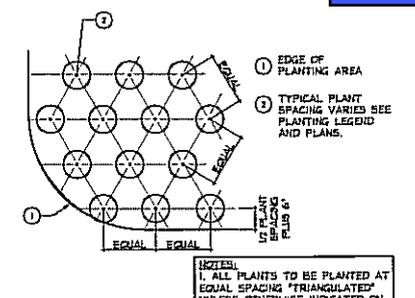
**E VINE PLANTING**  
KTA



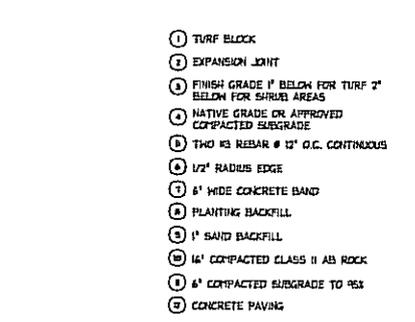
**D TREE STAKING (DOUBLE)**  
KTA



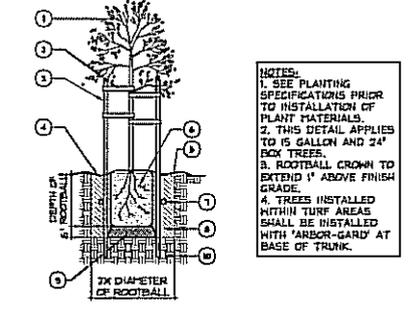
**C PALM TREE PLANTING**  
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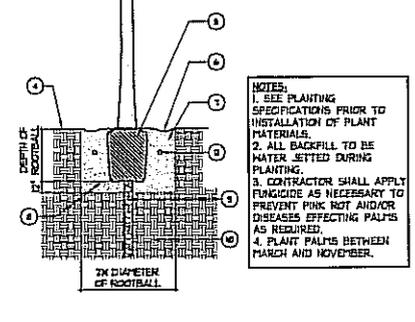
**A GROUNDCOVER SPACING**  
KTA



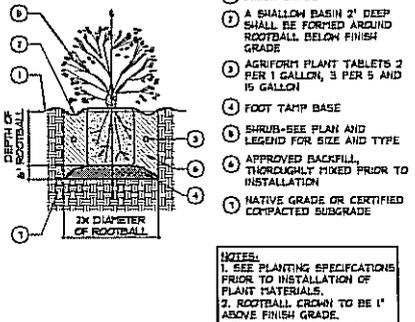
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SCALE: 1/4\"/>



**D TREE STAKING (DOUBLE)**  
KTA



**C PALM TREE PLANTING**  
KTA



**B SHRUB PLANTING**  
KTA

AA	11.10.07	Per Planning Comments
FD	11.28.07	Per Client Comments
AA	11.14.07	Per Client Comments
AA	11.14.07	Per City Comments
BY	DATE	REVISIONS

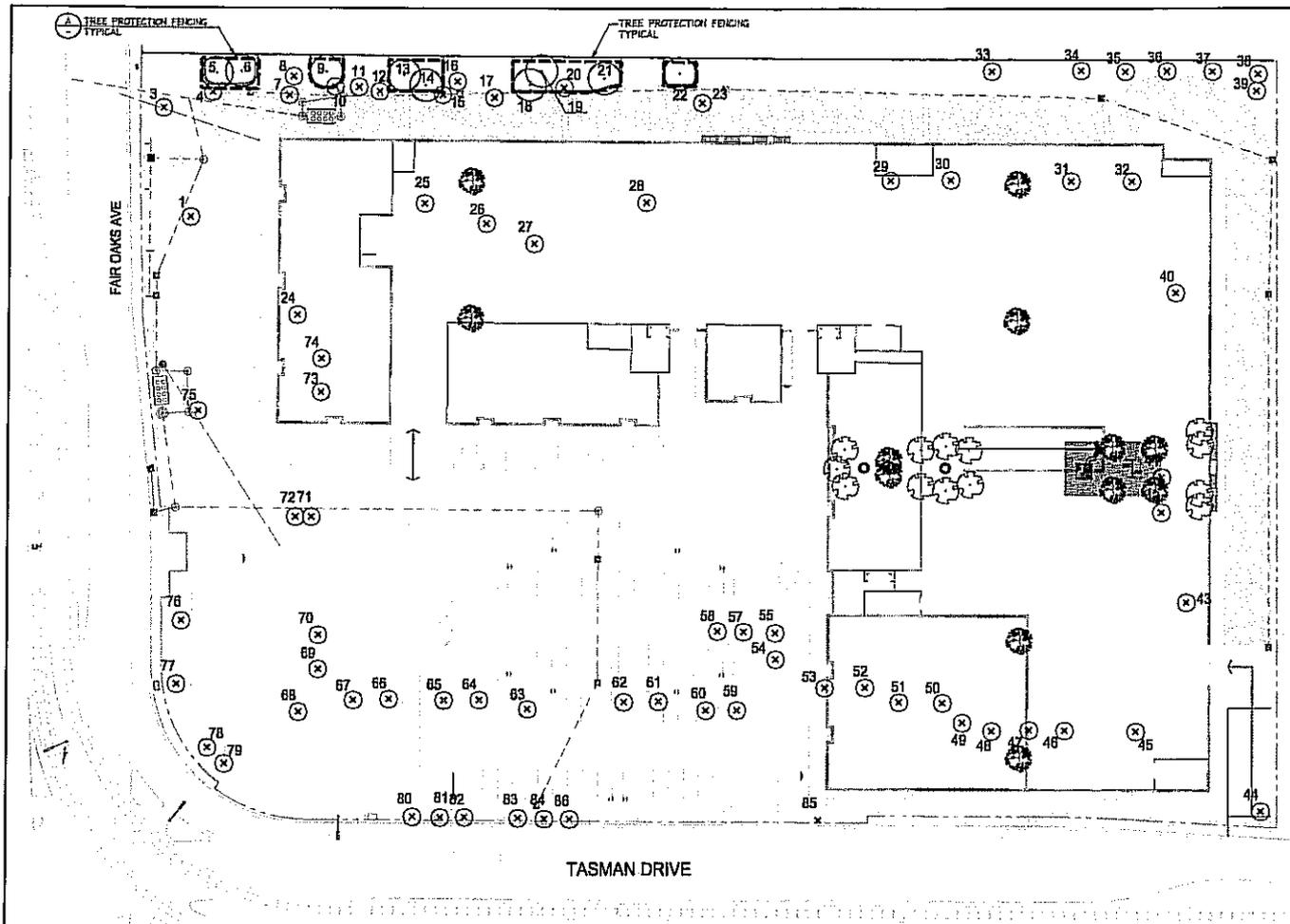
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Date: 04.24.07  
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Title: Engr. 0103  
File: 2002021001

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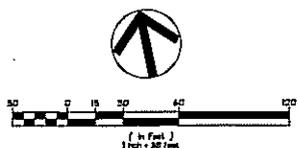


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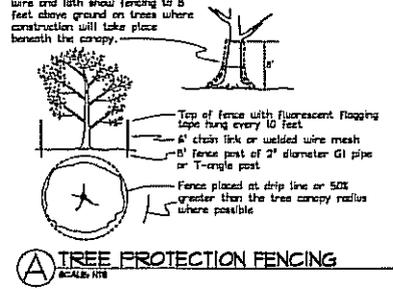
Tree #	Tree Name	Species	DBH	Height	Health	Notes
86	...	...	...	...	...	...
87	...	...	...	...	...	...
88	...	...	...	...	...	...
89	...	...	...	...	...	...
90	...	...	...	...	...	...
91	...	...	...	...	...	...
92	...	...	...	...	...	...
93	...	...	...	...	...	...
94	...	...	...	...	...	...
95	...	...	...	...	...	...
96	...	...	...	...	...	...
97	...	...	...	...	...	...
98	...	...	...	...	...	...
99	...	...	...	...	...	...
100	...	...	...	...	...	...

**LEGEND:**  
 85 TREE NUMBER, SEE REPORT.  
 (X) LOCATION OF EXISTING TREE TO BE REMOVED  
 (O) LOCATION OF EXISTING TREE TO PROTECT IN PLACE  
 --- TREE PROTECTION FENCING

**NOTES:**  
 A. # 2 DOES NOT EXIST (NUMBERED ON TREE SURVEY)  
 B. # 56 UNTAGGED TREE TO BE REMOVED  
 C. TREE REPORT PREPARED BY URBAN TREE MANAGEMENT, DATED: MARCH 13, 2007.



**NOTES:**  
 Construction period protection for trees should be provided before grading or other equipment is allowed on the property. When construction is to take place beneath a tree canopy on one side, the fence should be sited 2 to 3 feet beyond that construction, but between construction and the tree trunk. If construction or paving is to take place throughout the area beneath canopy, and drip line fencing is not practical, snow fencing should be used to protect the trunks from damage.  
**SNOW FENCING:** Three layers of wire and both snow fencing to 3 feet above ground on trees where construction will take place beneath the canopy.



BY	DATE	REVISIONS
AA	12/08	Per Planning Comments
JC	12/07	Per Client Comments
AA	11/07	Per Client Comments
AA	03/07	Per City Comments

**ESSEX**  
PROPERTY TRUST, INC.

Date: 04.24.07  
 Scale: 1/2" = 1'-0"  
 Designed: JMW/MS  
 Drawn: JMW/MS  
 Checked: WBS  
 Proj. Eng: DRH  
 File: 000007P.DWG

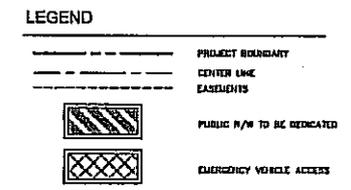
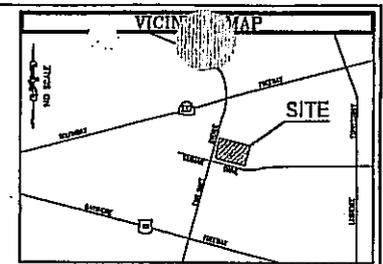
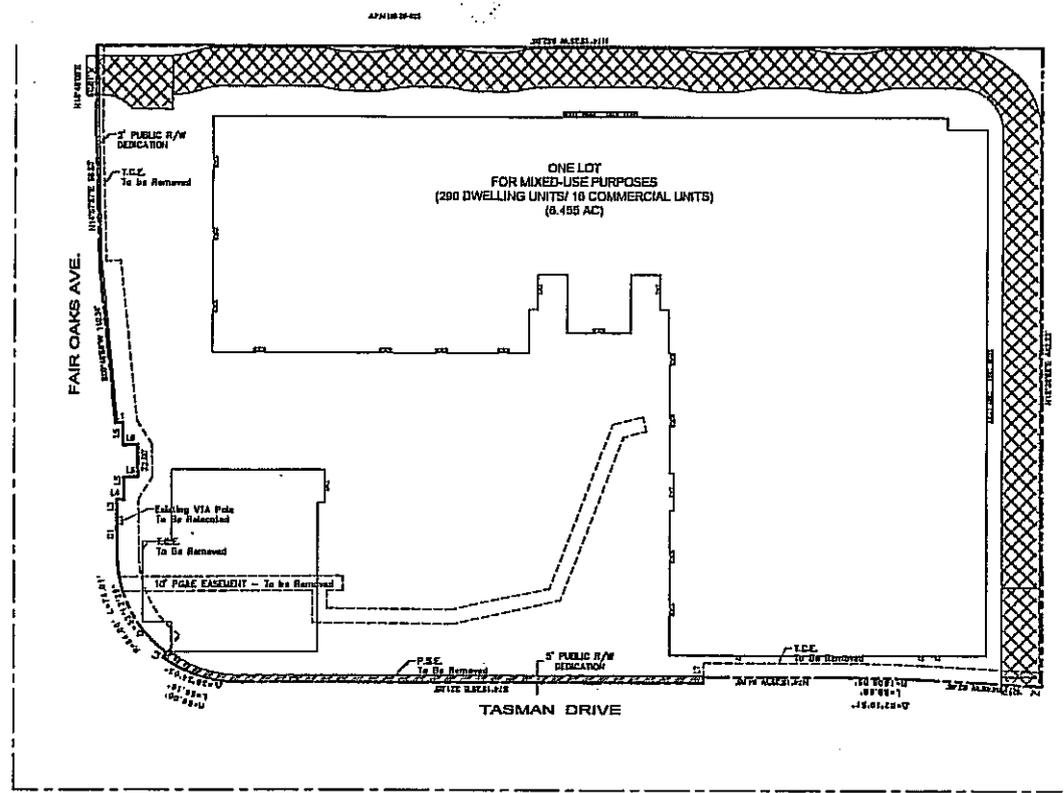
**HMH**  
LANDSCAPE ARCHITECTURE  
 1178 Colwood Pl  
 San Jose, CA 95128  
 ph: (408) 497-0200  
 fx: (408) 497-0202  
 www.hmhlandscape.com

SPECIAL DEVELOPMENT PERMIT  
 TREE REMOVAL PLAN  
 615 Tasman Drive, Sunnyvale, California

Sheet  
L-7  
 JOB NUMBER  
3620.00

**GENERAL NOTES**

- PROJECT NAME: TASMAN DRIVE
- OWNERS: 4. TASMAN/DELA ADVISORS LLC  
220 E 42ND ST 27TH FLOOR, NEW YORK NY
- SUBMITTER: ESSEX PROPERTY TRUST, INC.  
825 EAST MEADOW DRIVE  
PALO ALTO, CALIFORNIA 94303
- ENGINEER: HMM, INCORPORATED  
1570 DAKLAND ROAD, SUITE 200  
SAN JOSE, CA 95131  
(408) 487-2300  
DANNY HATHORN, P.E. #26810
- STREET LOCATION: 615 TASMAN DRIVE
- EXISTING ZONING: CS/PD
- EXTENSIVE OF DESIGNATION: MIXED USE
- EXISTING USE: INDUSTRIAL
- PROPOSED USE: MIXED USE
- WATER SUPPLY: CITY OF SUNNYVALE
- SEWAGE DISPOSAL: SUNNYVALE WATER POLLUTION CONTROL PLANT
- ASSESSOR'S PARCEL NO.: 510-25-03A
- STREET TREES: SHALL CONFORM TO CITY SUNNYVALE STANDARDS
- TOTAL SITE AREA: 8.455 ACRES (GROSS)
- PROPOSED DEDICATION: 34.00 ACRES
- NET SITE AREA: 8.455 NET ACRES
- PROPOSED LOTS: 1 LOT FOR MIXED USE
- MULTIPLE PARCEL MAPS MAY BE FILED PRIOR TO THE EXPARATION OF THE TENTATIVE MAP.
- IF ANY PORTION OF THE STRUCTURE(S) ARE GREATER THAN 150 FEET OF TRAVEL DISTANCE FROM THE CENTERLINE OF THE ROADWAY CONTAINING PUBLIC FIRE HYDRANTS, AN APPROVED RESIDENTIAL FIRE SPRINKLER SYSTEM THROUGHOUT ALL PORTIONS OF THE BUILDING WILL BE REQUIRED.
- REQUIRED ACCESS ROADS, UP THROUGH FIRST LIFT OF ASPHALT, SHALL BE INSTALLED AND ACCEPTED BY THE FIRE DEPARTMENT FROM THE START OF CONSTRUCTION. SOIL CONDITIONABLE MATERIALS SHALL NOT BE DELIVERED TO THE SITE UNTIL INSTALLATION IS COMPLETE. DURING CONSTRUCTION, EMERGENCY ACCESS ROADS SHALL BE MAINTAINED CLEAR AND UNIMPEDED. NOTE THAT DRAINAGE PERMIT ISSUANCE MAY BE WITHHELD UNTIL INSTALLATION IS COMPLETED.
- APPROVED NUMBERS ON ADDRESSES SHALL BE PLACED ON ALL NEW AND EXISTING BUILDINGS IN SUCH A POSITION AS TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY. NUMBERS SHALL CONTRAST WITH THEIR BACKGROUND.
- ROOF DRAINAGE LEADERS ARE TO BE DISCHARGED ONTO ENERGY DISSIPATORS (SPRINK BLOCKS) WHICH ARE DESIGNED TO SPREAD OUT THE RAIN WATER SO THAT IT ENTERS LANDSCAPE AREAS AS SHEET FLOW. RUNOFF FROM THE SITE SHOULD NOT BE COLLECTED INTO A PIPE SYSTEM, CONCENTRATED AND DISCHARGED DOWN SLOPE. CONTROL OFF-SITE DRAINAGE FLOWING TO THE SITE UNLAWFULLY. NO IMPROVEMENTS SHALL OCCUR ON DRAIN SUBJECT TO THE DIRECTION OF AN ADJACENT, DOWN STREAM OR DOWN SLOPE PROPERTY. RETAINING WALLS SHALL INCLUDE PROVISIONS FOR DRAINAGE.
- ANY EXISTING SANITARY SEWER LATERAL PROPOSED TO BE REUSED MUST BE RELEASED BY CITY OF SUNNYVALE AND APPROVED BY THE CITY BEFORE REUSE. A SANITARY LATERAL CLEAN-OUT SHALL BE INSTALLED AT THE PROPERTY LINE. IF ONE DOES NOT ALREADY EXIST WITHIN TWO (2) FEET OF THE PROPERTY LINE. ANY TRENCHING WITH THE DRAINAGE OF ALL TRENCH SHALL BE LAID OUT AND SO NOTED ON THE PLANS. ALL UTILITIES SERVING THE SITE SHALL BE UNDERGROUND.
- STREET LIGHTS WILL BE INSTALLED PER THE REQUIREMENTS OF THE CITY OF SUNNYVALE, DIRECTOR OF PUBLIC WORKS.
- FINAL GRADING PLANS SHALL INCLUDE A COMPLETE EROSION CONTROL PLAN. INTERIM EROSION CONTROL MEASURES TO BE CARRIED OUT DURING CONSTRUCTION AND BEFORE INSTALLATION OF THE FINAL LANDSCAPING SHALL BE INCLUDED. INTERIM EROSION CONTROL METHODS SHOULD INCLUDE Silt Fences OR STRAIN DALE COES (WITH LOCATION AND DETAILS) AND THE CITY STANDARDS SPECIFIC SPECIFICATION.
- DEMOLITION OF EXISTING STRUCTURES TO BE COMPLETED AS PART OF THE DEMOLITION PERMIT PROCESS AND PRIOR TO THE ACQUISITION OF FINAL BUILDING PERMIT APPROVAL.
- THIS SUBMISSION WILL CONFORM TO THE STREET TREE PLAN OF THE CITY OF SUNNYVALE.
- WALKWAYS, OPEN SPACE, AND PRIVATE RIGHT OF WAY CONSISTENT WITH THE APPROVED SPECIAL DEVELOPMENT PERMIT WILL BE OVERLAPPED WITH A COMMON ACCESS EASEMENT FOR THE BENEFIT OF THE DASH RESIDENTS.
- DEMOLITION PERMITS MAY BE ISSUED PRIOR TO ISSUANCE OF FINAL BUILDING PERMITS AND RECORDED OF FINAL MAP.



CURVE TABLE				TANGENT TABLE			
Sta	Radius	Delta	Length	Sta	Bearing	Length	Sta
0+00	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00
0+15.71	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00
0+31.42	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00
0+47.13	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00
0+62.84	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00
0+78.55	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00
0+94.26	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00
1+10.00	100'	90°00'	157.08'	1+00	S 89°20'15\"	3.00'	1+00



ATTACHMENT  
 Page 1 of 1

Δ		
Δ		
Δ		
Δ		
Δ	FD 12/07	Per Client Comments
Δ	BY DATE	REVISIONS

**ESSEX**  
PROPERTY TRUST, INC.

Date: 04/12/07  
 Scale: 1"=40'  
 Designed: AA  
 Drawn: FC  
 Checked: RHA  
 Proj. Engr: GMR  
 File: 342200121

**HMM**  
 ENGINEERS  
 San Jose  
 (408) 487-2300  
 Gilroy  
 (408) 846-0767  
 www.hmm-engineers.com

Vesting Tentative Tract Map for  
 Mixed-Use Purposes

Sunnyvale PTXX-XXX California

Sheet No. 3620.00

## **1. Attain the objectives and purposes of the General Plan of the City of Sunnyvale.**

**A.1.d** Study increasing the density of residential areas near transit stops and along major transportation corridors in conjunction with regional transportation plans.

**C3.2** Integrate the use of land and the transportation system.

**C3.2.1** Allow land uses that can be supported by the planned transportation system.

The proposed project is directly adjacent to the VTA Light Rail line and by significant bus routes, a bus stop and shelter are located along the project's Fair Oaks frontage. The proposed commercial and residential uses will complement the existing transit opportunities.

Transportation efficiency has become a high priority in many communities throughout the Bay Area, and is an important goal of the Sunnyvale General Plan. As traffic congestion and pollution steadily increase, the trend in urban infill has been to develop high-density housing in close proximity to transit lines to encourage public transit usage. The Fair Oaks VTA light rail station located directly in front of the site makes this the ideal location for this type of high-density residential development. The light rail serves most of Silicon Valley from Mountain View to San Jose, and connects to other major modes of public transportation. Residents of this development would have the ability to easily commute and travel throughout the valley without ever having to use their vehicles.

**A.3.a** Maintain provisions of the zoning code that permit housing to be constructed in commercial districts after planning review.

The project site contains a mixed-use zoning designation of C2/PD, which allows for high density residential development in conjunction with residential development. The mix of uses will be reviewed through the Special Development Permit process with the Planning Department.

**Policy A.2** All new residential developments should build at least 75 percent of permitted zoning density.

The proposed development has a density of 51.28 dwelling units to the acre. The project meets the density requirements for the site.

**R1.10.3** Encourage mixed uses near transit centers.

**C3.2.3** Encourage mixed use developments that provide pedestrian scale and transit oriented services and amenities.

The proposed site is located adjacent to the Tasman Light Rail station. A Santa Clara County Valley Transit Authority bus stop and shelter are located at the Fair Oaks project edge. The proposed project will place housing along the transit lines. The proposed commercial uses will serve the surrounding residential uses.

The incorporation of new commercial space in this project will help to boost the local economy by providing new retail opportunities and generating sales tax revenue for the City. Additionally it will give local residents access to new services that are not currently available in the area, and will create a comfortable gathering place for the community.

**Goal C2** Ensure ownership and rental housing options in terms of style, size, and density that are appropriate and contribute positively to the surrounding area.

The project will place 331 units of rental housing near a variety of other types of existing residential uses; including condominium townhomes, attached residential units, and mobile homes. The proposed units will contribute to the variety and total number of housing units in the vicinity.

The General Plan seeks to foster the expansion of the housing supply, while maintaining diversity in the type, size, and tenure of the product available for current and future residents. Over the last five years, the area surrounding the development site has undergone tremendous transformation with the addition of a variety of new townhome developments. The addition of a new apartment community would further diversify the neighborhood housing supply.

**C1.2** Encourage nodes of interest and activity, such as parks, public open spaces, well planned development, mixed use projects, and other desirable uses, locations and physical attractions.

As described above, the proposed project will appropriately place housing units and commercial uses on the site. The project will provide desirable services in a comfortable architectural setting that provides street-adjacent commercial uses fostering a pedestrian friendly environment.

**C3.2.2** Minimize driveway curb cuts, and require coordinated access when appropriate.

The project minimizes curb cuts as much as possible by providing access to both the commercial and residential uses on the site via the same access points from the street, rather than having separate access for the two distinct uses.

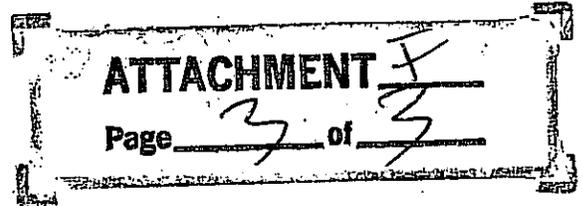
**C3.2.5** Study potential transit station mixed use development.

As stated in the above points, the proposed project furthers the goal of placing a mix of uses near existing transit stations.

## **2. Ensure that the general appearance of proposed structures, or the uses to be made of the property to which the application refers, will not impair either the**

# ESSEX

PROPERTY TRUST, INC.



**orderly development of, or the existing uses being made of, adjacent properties.**

There are a variety of uses in the immediate vicinity of the project. The proposed project will complement the existing residential uses in the area. The project proposes to place neighborhood serving, street-adjacent retail near the existing residential uses and near the existing light rail station.